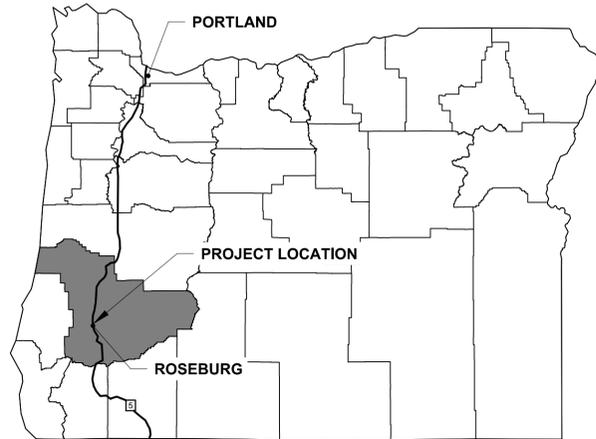
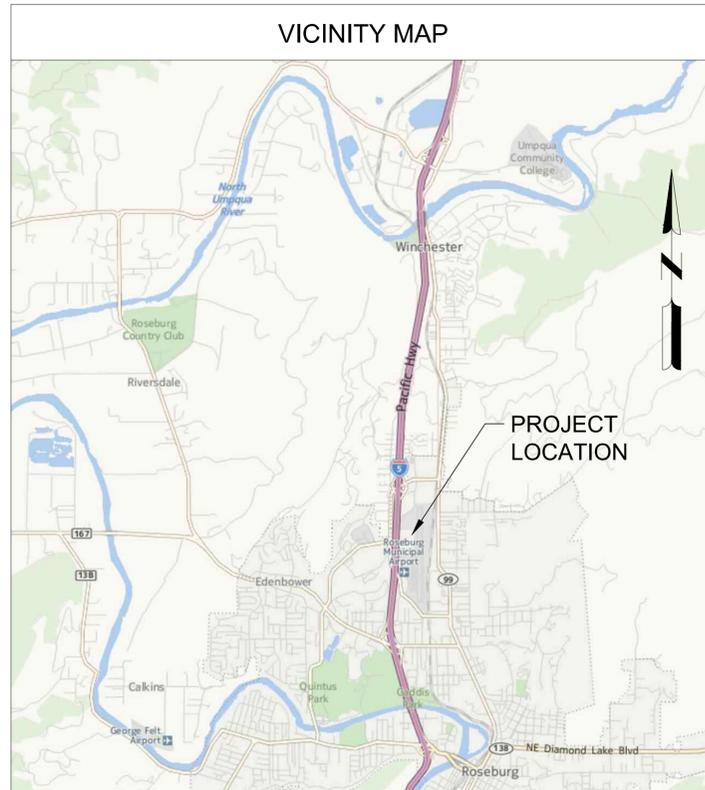


# ROSEBURG REGIONAL AIRPORT

TAXIWAY A EXTENSION  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470  
 AIP NO. 3-41-0054-030-2024  
 PROJECT #22GR18  
 APRIL 18, 2024



DOUGLAS COUNTY, OREGON



UTILITY CONTACTS	
CITY OF ROSEBURG .....	RYAN HERINCKX (541) 492-6884
PACIFIC POWER .....	INSPECTIONS (541) 580-2945
PACIFIC POWER .....	EMERGENCY (425) 392-6412
CENTURYLINK .....	EMERGENCY (800) 778-9140
AVISTA .....	EMERGENCY (541) 613-0888
SPRINT.....	(800) 521-0579

**811**

Know what's below.  
 Call before you dig.

**ATTENTION:**  
 OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH 952-001-0100. YOU MAY OBTAIN COPIES OF THE RULES AT DIGSAFELYOREGON.COM

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Mead and Hunt, Inc.  
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ROSEBURG REGIONAL AIRPORT  
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 DRAWN BY: SDB  
 CHECKED BY: RJB  
 DO NOT SCALE DRAWINGS

SHEET CONTENTS  
 COVER SHEET

SHEET NO. 1 of 40

G-001

**FEATURES**

	(E) RUNWAY LIGHTING
	GEOTECHNICAL TEST BORE HOLE
	(E) CURB INLET (DI)
	(E) CATCH BASIN (CB)
	(E) SANITARY CLEANOUTS
	(E) WATER VALVE
	(E) SANITARY MANHOLE (MH)
	(E) STORM MANHOLE
	(E) POWER POLE
	(E) PHONE PED
	(E) LIGHT/SIGNAL POLE
	(E) MAIL BOX
	(E) WATER SERVICE SHUTOFF
	(E) ELECT. METER BASE
	(E) SAN. HOOKUP
	(E) TRANSFORMER
	(E) ELECT. BOX
	(E) UNDERGROUND VAULT
	(E) RUNWAY/TAXIWAY LIGHT
	(N) FULL STRENGTH HMA PAVEMENT
	(N) SHOULDER AB PAVEMENT
	(N) PCC SIDEWALK
	CONCRETE RETAINING WALL AND BLAST DEFLECTOR
	CONTROL POINT
	BENCHMARK
	(N) STORM DRAINAGE INLET
	(N) STORM MANHOLE
	(N) CLEANOUT
	(N) ELEVATED TAXIWAY EDGE LIGHT (BLUE)
	(N) ELEVATED RUNWAY EDGE LIGHT (RED)
	(N) ELEVATED RUNWAY END LIGHT (RED/YELLOW)
	(N) AIRFIELD ELECTRICAL HANDHOLE
	(N) AIRFIELD JUNCTION BOX
	(N) GATE
	(E) GATE

**LINETYPES**

	E	LOW-PROFILE BARRICADES
	FOC	(E) POWER (UNDERGROUND)
	G	(E) FIBER OPTIC (DFN)
	R/W	(E) GAS
	X	(E) RIGHT-OF-WAY
	W	(E) CHAIN LINK FENCE
	SD	(E) WATER LINE
	SS	(E) STORM SEWER
	SS	(E) SANITARY SEWER
	ROFA	(E) EDGE OF PAVEMENT
	TOFA	RUNWAY OBJECT FREE AREA
	RSA	TAXIWAY OBJECT FREE AREA
	TSA	RUNWAY SAFETY AREA
	TSA	TAXIWAY SAFETY AREA
	OFZ	TAXIWAY SAFETY AREA
	OFZ	TAXIWAY OBJECT FREE AREA
	TOFA	OBSTACLE FREE ZONE
	G	(N) GAS LINE
	ST	(N) STORM LINE
	UD	(N) SUBDRAIN LINE
	E	(N) ELECTRICAL LINE
		TO BE REMOVED
		(N) SEDIMENT FENCE
		(N) EDGE OF PAVEMENT
		(N) EDGE OF SHOULDER
		(N) EDGE OF BLAST PAD

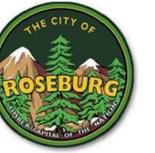
**ABBREVIATIONS**

AB	AGGREGATE BASE
APPROX	APPROXIMATE
AWG	AMERICAN WIRE GAUGE
BB	BASE BID SCHEDULE WORK
BMPS	BEST MANAGEMENT PRACTICES
CL	CENTERLINE
CLSM	CONTROLLED LOW-STRENGTH MATERIAL
CO	CLEANOUT
CONC	CONCRETE
DI	DRAIN INLET
DIA	DIAMETER
(E), EX	EXISTING
E	ELECTRICAL LINE
ELEV	ELEVATION
EOP	EDGE OF PAVEMENT
FT	FEET
G	GAS LINE
HMA	HOT-MIX ASPHALT
INV	INVERT
L	LENGTH
LB	POUND
LF	LINEAL FEET
LT	LEFT
MAX	MAXIMUM
MH	MANHOLE
MIN	MINIMUM
(N)	NEW
NDGS	NATIONAL GEODETIC SURVEY
NGVD	NATIONAL GEODETIC VERTICAL DATUM
NTS	NOT TO SCALE
OFZ	OBSTACLE FREE ZONE
O/S	OFFSET
ODOT	OREGON DEPARTMENT OF TRANSPORTATION
POC	POINT OF CURVATURE
PCC	PORTLAND CEMENT CONCRETE
PCF	POUNDS PER CUBIC FOOT
PERF	PERFORATED
PNT	POINT
POC	POINT OF CURVE
PT	POINT OF TANGENCY
PVC	POLYVINYL CHLORIDE
PVI	POINT OF VERTICAL INTERSECTION
PVIE	POINT OF VERTICAL INTERSECTION ELEVATION
PVIS	POINT OF VERTICAL INTERSECTION STATION
PVT	POINT OF VERTICAL TANGENT
R	RADIUS
RCP	REINFORCED CONCRETE PIPE
RED	RUNWAY EDGE LIGHT
REL	RUNWAY END LIGHT
ROFA	RUNWAY OBJECT FREE AREA
RSA	RUNWAY SAFETY AREA
RT	RIGHT
RWA	RESTRICTED WORK AREA
RWY	RUNWAY
S	SLOPE
SS	SANITARY SEWER
ST	STORM SEWER LINE
STA	STATION
STRM	STORM
TEL	TAXIWAY EDGE LIGHT
TOFA	TAXIWAY OBJECT FREE AREA
TSA	TAXIWAY SAFETY AREA
TWY	TAXIWAY
UT	UNDERGROUND TELEPHONE
W	WATER LINE
WA	WORK AREA
W/	WITH
WV	WATER VALVE

X:\1824200\221217\_01\TECH\CAD\G-002 GENERAL NOTES - SYMBOLS & ABBREVIATIONS.DWG  
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**Mead & Hunt**

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**ROSEBURG REGIONAL AIRPORT  
TAXIWAY A EXTENSION  
3896 NW STEWART PARKWAY  
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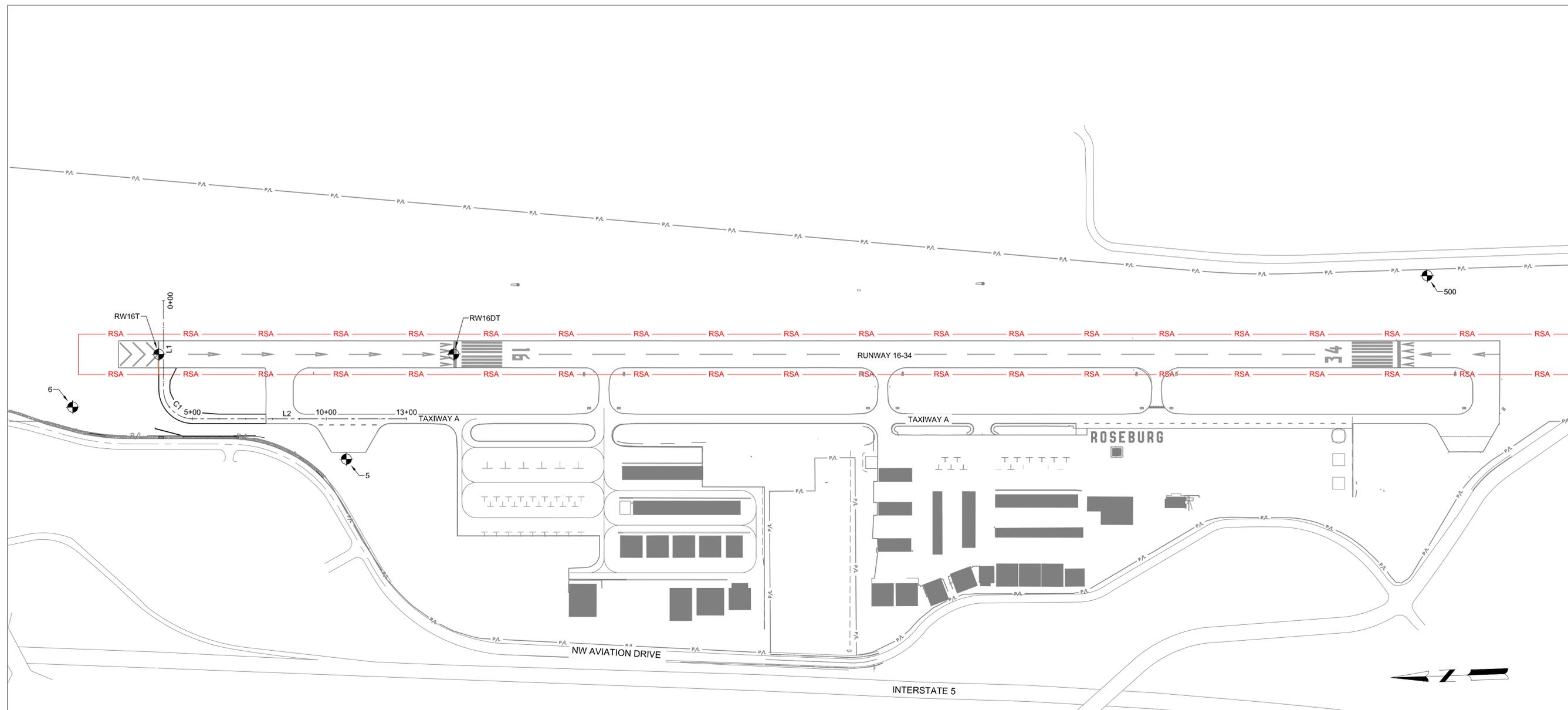
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GENERAL NOTES -  
SYMBOLS - LEGEND &  
ABBREVIATIONS

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**G-002**



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**SURVEY CONTROL POINTS**

POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION
5	587935.29	4160122.48	522.18	3/8" IRON ROD W/ YELLOW PLASTIC CAP "LMSI CONTROL"
6	588949.0470	4160349.6260	527.765	3/8" IRON ROD W/ YELLOW PLASTIC CAP "LMSI CONTROL"
RW16T	588621.5981	588621.5981	533.365	CP MAG W. WASHER
RW16DT	587522.2011	4160499.6276	523.753	CP MAG W. WASHER
500	4160671.91	583884.10	501.81	CONCRETE POST W/ BRONZE USGS CAP

**NOTES:**

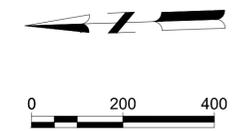
- SURVEY WAS PERFORMED IN DEC 2022 BY LAND MARK SURVEYING INC., ROSEBURG, OR. IN ACCORDANCE WITH GENERAL PROVISION 50-07, CONTRACTOR SHALL CHECK CONTROL POINTS FOR HORIZONTAL AND VERTICAL ACCURACY AND CERTIFY IN WRITING TO THE RPR THAT THE CONTRACTOR CONCURS WITH SURVEY CONTROL ESTABLISHED FOR THE PROJECT. IN THE EVENT OF A DISCREPANCY, CARE SHOULD BE TAKEN TO DETERMINE WHICH POINT (OR POINTS) HAVE BEEN COMPROMISED. POINT 500 SHALL BE PARAMOUNT IN THE NETWORK FOR THE CONTRACTOR TO RE-ESTABLISH ELEVATIONS SHOULD IT BECOME NECESSARY.
- THE LOCATION OF THE EXISTING UNDERGROUND UTILITY FACILITIES HAS NOT BEEN RESEARCHED. THE LOCATION OF UTILITIES ON THIS SURVEY ARE BASED ON VISIBLE EVIDENCE OF FACILITIES ONLY AS MARKED BY THE CITY OF ROSEBURG AND UTILITY COMPANIES. THE SURVEYOR ASSUMES NO RESPONSIBILITY FOR THE DELINEATION OF SUCH UNDERGROUND UTILITIES, NOR FOR THE EXISTENCE OF BURIED OBJECTS WHICH ARE NOT SHOWN ON THIS PLAN.
- HORIZONTAL DATUM - OREGON STATE PLANE COORDINATE SYSTEM, SOUTH ZONE NAD 83(2007) - INTERNATIONAL FEET  
VERTICAL DATUM - NAVD 88, UTILIZING NGS MONUMENT 500

**ALIGNMENT LINE DATA**

LINE #	LENGTH	STATION START	STATION END	NORTHING START	EASTING START	NORTHING END	EASTING END
L1	330.00	0+00.00	3+30.00	588597.5520	4160734.9068	588597.5520	4160405.0862
L2	797.21	5+02.79	13+00.00	588502.1186	4160291.5193	588502.1186	4160265.2352

**ALIGNMENT CURVE DATA**

#	RADIUS	Δ	TANGENT	LENGTH	CENTER POINT NORTHING	CENTER POINT EASTING	PC NORTHING	PC EASTING	PT NORTHING	PT EASTING
C1	110.00'	90° 00' 00"	110.00	172.79'	588498.492	4160401.459	588608.432	4160405.086	588502.119	4160291.519



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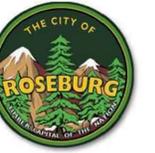


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SHEET CONTENTS  
SURVEY CONTROL PLAN

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**G-021**



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

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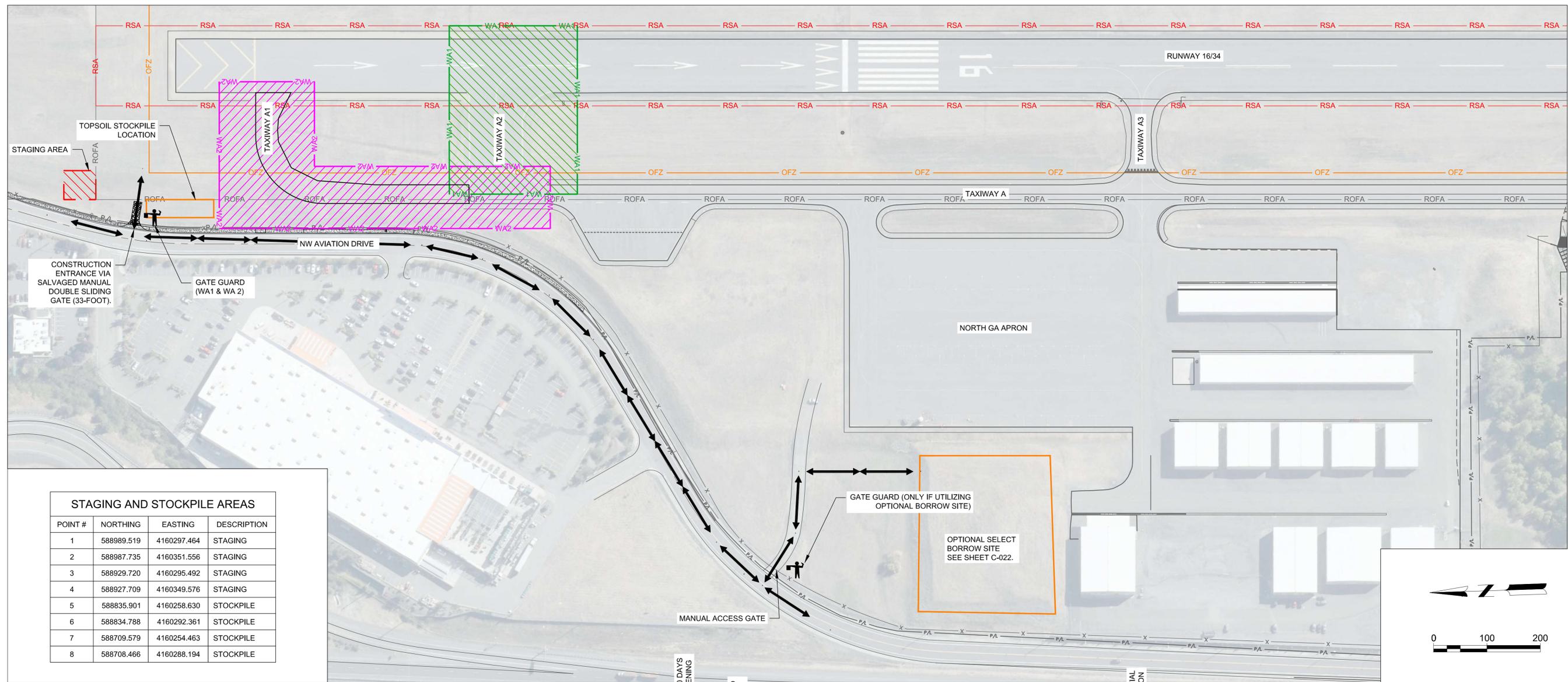


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SHEET CONTENTS  
CONSTRUCTION OPERATIONS & PHASING PLAN - OVERALL

SHEET NO. 4 of 40

### G-080



STAGING AND STOCKPILE AREAS			
POINT #	NORTHING	EASTING	DESCRIPTION
1	588989.519	4160297.464	STAGING
2	588987.735	4160351.556	STAGING
3	588929.720	4160295.492	STAGING
4	588927.709	4160349.576	STAGING
5	588835.901	4160258.630	STOCKPILE
6	588834.788	4160292.361	STOCKPILE
7	588709.579	4160254.463	STOCKPILE
8	588708.466	4160288.194	STOCKPILE

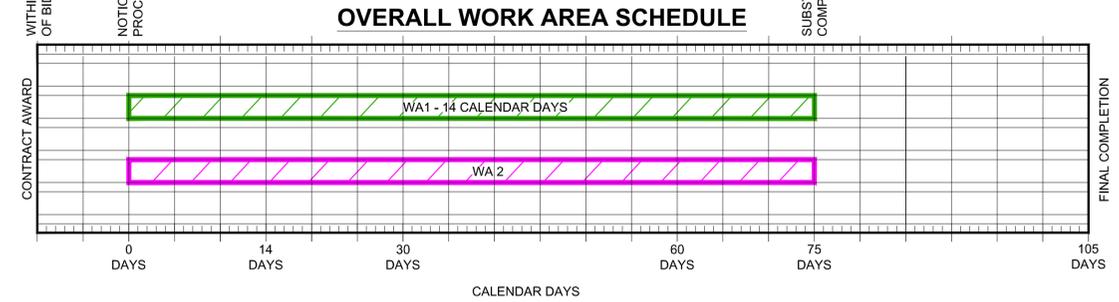
	MAJOR WORK ELEMENTS	IMPACT TO AIRPORT OPERATIONS
<b>WORK AREA 1</b>	-REMOVE TAXIWAY A2 AND MISCELLANEOUS INFRASTRUCTURE	-RUNWAY 16/34 CLOSED FOR 14 CALENDAR DAYS BETWEEN 8AM AND 4:30PM LOCAL TIME. SEE G-081.
<b>WORK AREA 2</b>	-EXTEND TAXIWAY ALPHA 400' NORTH. CONSTRUCT CONNECTOR TAXIWAY A1. INSTALL RETAINING WALL, JET BLAST FENCE, PAVEMENT UNDERDRAINS, PAVEMENT MARKINGS, RUNWAY AND TAXIWAY EDGE LIGHTS, TAXIWAY GUIDANCE SIGNS.	-MODIFIED DECLARED DISTANCES FOR RUNWAY 16/34 DURING CONSTRUCTION. TAXIWAY A NORTH OF GA APRON CLOSED. SEE G-082.

#### AIRPORT INFORMATION

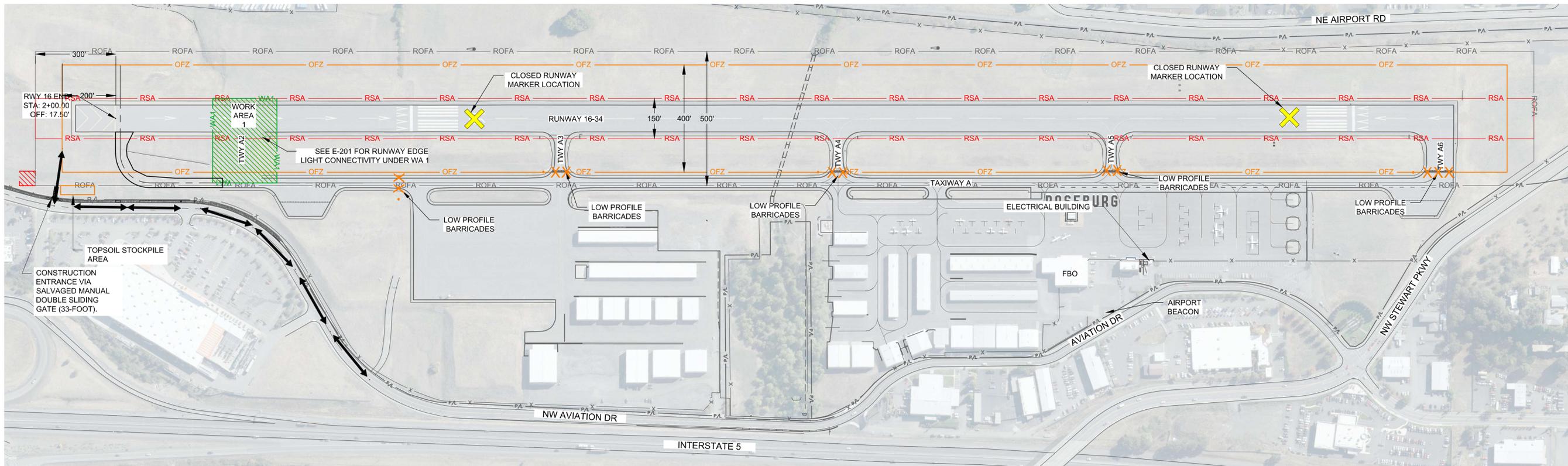
AIRPORT REFERENCE CODE (ARC): B-II  
TAXIWAY DESIGN GROUP (TDG): II  
CRITICAL AIRCRAFT: CESSNA CITATION XLS  
TAXIWAY OBJECT FREE AREA: 131 FEET (65.5 FEET FROM TAXIWAY C/L)  
COMMON TRAFFIC ADVISORY FREQUENCY (CTAF): 122.8 MHz  
RUNWAY 16 DISPLACED THRESHOLD: N43° 14' 35.4425", W123° 21' 21.0868"  
RUNWAY 34 THRESHOLD: N43° 13' 56.9054", W123° 21' 21.0315"

#### CONSTRUCTION PHASING GENERAL NOTES:

- ALL WORK SHALL BE SUBSTANTIALLY COMPLETED WITHIN 75 CALENDAR DAYS.
- DUE TO FEDERAL FUNDING AVAILABILITY, THE CITY ANTICIPATES CONTRACT AWARD IN SUMMER/FALL 2024. THE CONTRACTOR WILL HAVE THE OPTION TO PERFORM CONSTRUCTION IN 2024 OR 2025, OR A COMBINATION OF BOTH. A TEMPORARY SUSPENSION OF WORK, IN ACCORDANCE WITH GP 80-06, WILL BE ISSUED IF THE CONTRACTOR ELECTS TO PERFORM WORK IN BOTH 2024 AND 2025.  
IF THE CONTRACTOR ELECTS TO BEGIN CONSTRUCTION IN 2024, THE FOLLOWING CONDITIONS APPLY:  
WORK ELEMENTS LOCATED OUTSIDE THE RUNWAY OBSTACLE FREE ZONE (OFZ) AND TAXIWAY OBJECT FREE AREA (TOFA) DO NOT IMPACT AIRPORT OPERATIONS AND CAN BE PERFORMED WITHOUT RUNWAY OR TAXIWAY CLOSURES. THESE ITEMS INCLUDE, BUT ARE NOT LIMITED TO, THE RETAINING WALL, JET BLAST DEFLECTOR, SIDEWALK, & PERIMETER FENCING. THESE ELEMENTS OF WORK MUST BE SUBSTANTIALLY COMPLETED PRIOR TO ISSUANCE OF A TEMPORARY SUSPENSION OF WORK.  
WORK ELEMENTS LOCATED WITHIN THE RUNWAY OBSTACLE FREE ZONE (OFZ) AND TAXIWAY OBJECT FREE AREA (TOFA) REQUIRE RUNWAY CLOSURE AND TEMPORARY PARTIAL RUNWAY CLOSURE. COMMENCEMENT OF WORK WITHIN THE OFZ AND TOFA WILL REQUIRE FINAL COMPLETION OF THE PROJECT IN ITS ENTIRETY IN 2024.  
IF THE CONTRACTOR ELECTS TO PERFORM CONSTRUCTION IN 2025, IT SHALL BEGIN NO LATER THAN APRIL 1, 2025, UNLESS OTHERWISE APPROVED BY THE CITY.
- ALL AIRCRAFT PAVEMENT SURFACES REMAIN OPEN AT ALL TIMES OTHER THAN THE DESIGNATED CLOSURES.
- THE CONTRACTOR SHALL NOT MOVE PERSONNEL, EQUIPMENT, OR MATERIALS ONTO THE AOA WITHOUT AUTHORIZATION FROM THE AIRPORT.
- THE CONTRACTOR SHALL NOT ENTER ANY ACTIVE RSA, TSA, TOFA, OR ACTIVE AIRPORT MOVEMENT AREA WITHOUT AUTHORIZATION FROM THE AIRPORT.
- DAILY SAFETY INSPECTION SHALL BE PERFORMED BY THE CONTRACTOR AS IDENTIFIED IN THE CSPP.
- A PROJECT SUPERINTENDENT OR DESIGNATED PROJECT REPRESENTATIVE SHALL BE ON-SITE AT ALL TIMES WHILE CREWS ARE IN THE FIELD. THIS INCLUDES A PRIME CONTRACTOR REPRESENTATIVE DURING ALL WORK BEING PERFORMED BY SUBCONTRACTORS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, DEPLOYING, MAINTAINING, RELOCATING, AND REMOVING THE LOW-PROFILE BARRICADES AS SHOWN ON THE PLANS, OR AS DIRECTED BY THE AIRPORT. LOW PROFILE BARRICADES SHALL BE PLACED END-TO-END OR INTERCONNECTED, OR AS DIRECTED BY THE AIRPORT.
- CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF THE CSPP. CONTRACTOR IS REQUIRED TO SUBMIT A SPDC, AS OUTLINED IN THE CSPP, AT THE PRE-CONSTRUCTION MEETING.
- MATERIAL STOCKPILES AND EQUIPMENT STORAGE IS PROHIBITED WITHIN ANY ACTIVE TAXIWAY OR RUNWAY OBJECT FREE AREA.
- TRAFFIC CONTROL SHALL BE IN PLACE PRIOR TO BEGINNING WORK IN THE AFFECTED WORK AREA. ALL SAFETY AND TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL AIRPORT OPENS AFFECTED TAXIWAY OR MOVEMENT AREA TO AIRCRAFT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE TRAFFIC CONTROL DEVICES THROUGHOUT THE DURATION OF THE CLOSURE.
- CONTRACTOR IS RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH CONSTRUCTION WATER.
- CONTRACTOR TO COORDINATE LOCKOUT-TAGOUT OF AIRFIELD CIRCUITS ON PAVEMENTS CLOSED TO AIRFIELD TRAFFIC. IN THE CASE OF PARTIAL TAXIWAY CLOSURES, CONTRACTOR IS RESPONSIBLE FOR INSTALLING TEMPORARY JUMPER CABLES PER NS-102 SPECIFICATIONS.
- AIRPORT GATES LEFT OPEN FOR CONSTRUCTION TRAFFIC MUST BE MONITORED BY A DESIGNATED GATE GUARD AT ALL TIMES.
- SIDEWALK CLOSURE REQUIREMENTS SHOWN ON G-084.



LEGEND:	
	CONTRACTOR ACCESS ROUTE
	RUNWAY SAFETY AREA
	RUNWAY OBJECT FREE AREA
	OBSTACLE FREE ZONE
	CONTRACTOR STAGING AREA
	GATE GUARD

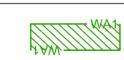


**PROJECT WORK AREA REQUIREMENTS:**

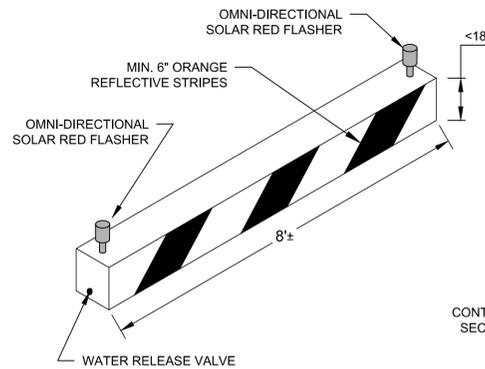
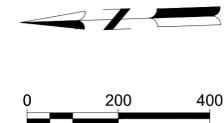
1. THE STATION/OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE TAXIWAY A ALIGNMENT.
2. ALL WORK CONTAINED IN THE PROJECT WORK AREA REQUIRES RUNWAY CLOSURE.
3. WORK AREA 1 SHALL BE SUBSTANTIALLY COMPLETED IN 14 CALENDAR DAYS. THE CONTRACTOR SHALL PROVIDE A SCHEDULE DURING THE PRE-CONSTRUCTION MEETING THAT IDENTIFIES THE INTENDED DATES AND SEQUENCE OF RUNWAY CLOSURE(S). START OF WORK AREA 2 MAY BE CONCURRENT WITH THE START OF WORK AREA 1.
4. DURING PERIODS OF RUNWAY CLOSURE, THE RUNWAY MAY ONLY BE CLOSED FROM 8:00 AM TO 4:30 PM LOCAL TIME.
5. OPEN TRENCHES AND EXCAVATIONS ARE ALLOWED TO REMAIN OPEN WHILE THE RUNWAY IS OPERATIONAL PROVIDED THEY ARE LOCATED OUTSIDE THE ACTIVE RUNWAY SAFETY AREA AND TAXIWAY OBJECT FREE AREA. HOWEVER, MATERIAL STOCKPILES AND EQUIPMENT STORAGE IS PROHIBITED WITHIN THE RUNWAY OBJECT FREE AREA AT ALL TIMES WHEN THE RUNWAY IS OPEN.
6. CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). CONTRACTOR REQUIRED TO SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) AS OUTLINED IN THE CSPP, AT THE PRE-CONSTRUCTION MEETING.
7. DAILY SAFETY INSPECTION SHALL BE PERFORMED BY THE CONTRACTOR AS IDENTIFIED IN THE CSPP.
8. A PROJECT SUPERINTENDENT OR DESIGNATED PROJECT REPRESENTATIVE SHALL BE ON-SITE AT ALL TIMES WHILE CREWS ARE IN THE FIELD. THIS INCLUDES A PRIME CONTRACTOR REPRESENTATIVE DURING ALL WORK BEING PERFORMED BY SUBCONTRACTORS.
9. TO PREPARE FOR THE PARTIAL RUNWAY CLOSURE REQUIRED FOR WORK AREA 2, CONTRACTOR MAY INSTALL TEMPORARY THRESHOLD LIGHT BASE CANS DURING THIS WORK AREA. EXISTING RUNWAY EDGE LIGHTING SHALL REMAIN OPERATIONAL IN THE EXISTING CONFIGURATION UNTIL THE ONE CALENDAR DAY RUNWAY CONVERSION DESCRIBED UNDER WORK AREA 2 IS PERFORMED.
10. EQUIPMENT HEIGHT SHALL NOT EXCEED 25-FEET, UNLESS OTHERWISE APPROVED BY THE CITY.

**RUNWAY CLOSURE REQUIREMENTS:**

1. ALL CONSTRUCTION SHALL BE IN COMPLIANCE WITH THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP).
2. RUNWAY 16-34 SHALL BE CLOSED WHENEVER PERSONNEL, MATERIALS, OR EQUIPMENT ARE WITHIN THE RUNWAY SAFETY AREA (RSA) OR THE RUNWAY OBSTACLE FREE ZONE (OFZ). THE CITY WILL ISSUE A NOTICE TO AIRMEN (NOTAM) DURING PERIODS OF RUNWAY CLOSURE.
3. THE CONTRACTOR SHALL NOT ENTER ANY ACTIVE RUNWAY SAFETY AREA (RSA), OBSTACLE FREE ZONE (OFZ), OR TAXIWAY OBJECT FREE AREA (TOFA) WITHOUT AUTHORIZATION FROM THE CITY.
4. AFTER THE NOTAM HAS BEEN ISSUED AND ACCESS TO THE RUNWAY HAS BEEN GRANTED BY THE CITY, CONTRACTOR SHALL IMMEDIATELY PLACE RUNWAY CLOSURE MARKERS ON BOTH ENDS OF THE RUNWAY.
5. THE CONTRACTOR SHALL DISABLE THE FOLLOWING CIRCUITS INSIDE THE ELECTRICAL BUILDING ONCE THE NOTAM HAS BEEN ISSUED AND BEFORE WORK MAY BEGIN: RUNWAY EDGE LIGHTING, TAXIWAY EDGE LIGHTING, RUNWAY 16 REIL, RUNWAY 34 REIL, RUNWAY 34 PAPI, AND WINDCONE.
6. PRIOR TO REOPENING RUNWAY 16-34, A FINAL INSPECTION OF THE ENTIRE RUNWAY AND RUNWAY SAFETY AREA WILL BE MADE TO ENSURE THAT ALL REQUIREMENTS HAVE BEEN MET AND THAT THE RUNWAY IS IN SAFE OPERATING CONDITION PRIOR TO REOPENING. THE CONTRACTOR SHALL COORDINATE WITH THE CITY TO ENSURE THAT ALL RUNWAY EDGE LIGHTS AND NAVAIDS ARE OPERATING CORRECTLY AND THAT CLOSED RUNWAY MARKERS HAVE BEEN REMOVED.
7. A PROJECT SUPERINTENDENT OR DESIGNATED PROJECT REPRESENTATIVE SHALL BE ON-SITE AT ALL TIME WHILE CREWS ARE IN THE FIELD. THIS INCLUDES A PRIME CONTRACTOR REPRESENTATIVE DURING ALL WORK BEING PERFORMED BY SUBCONTRACTORS.
8. LIQUIDATED DAMAGES FOR FAILURE TO COMPLETE WORK IN THE TIME PROVIDED ARE DEFINED IN SECTION 80-08 OF THE GENERAL CONTRACT PROVISIONS.

	MAJOR WORK ELEMENTS	IMPACT TO AIRPORT OPERATIONS
	<b>WORK AREA 1</b>	
	REMOVE TAXIWAY A2 AND MISCELLANEOUS INFRASTRUCTURE	-RUNWAY 16/34 CLOSED FOR 10 CALENDAR DAYS BETWEEN 8AM AND 4:30PM LOCAL TIME. -RUNWAY EDGE LIGHTING - OUT OF SERVICE -TAXIWAY EDGE LIGHTING - OUT OF SERVICE -RUNWAY 16 REIL - OUT OF SERVICE -RUNWAY 34 PAPI - OUT OF SERVICE
	INSTALL TEMPORARY THRESHOLD LIGHT BASE CANS FOR PARTIALLY CLOSED RUNWAY NEEDED UNDER WORK AREA 2. SEE NOTE 10.	

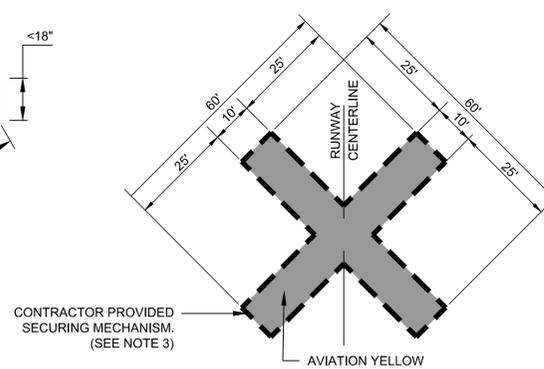
LEGEND:	
	CONTRACTOR ACCESS ROUTE
	RUNWAY SAFETY AREA
	RUNWAY OBJECT FREE AREA
	OBSTACLE FREE ZONE
	CONTRACTOR STAGING AREA
	GATE GUARD
	LOW-PROFILE BARRICADES
	RUNWAY CLOSURE MARKER



**1 LOW PROFILE BARRICADE DETAIL**  
NOT TO SCALE

**LOW PROFILE BARRICADE NOTES:**

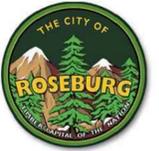
1. LOW PROFILE BARRICADES AND OMNIDIRECTIONAL FLASHERS WILL BE FURNISHED BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR DEPLOYING, RELOCATING, AND REMOVING LOW PROFILE BARRICADES IN ACCORDANCE WITH THE PHASING PLANS OR AS OTHERWISE DIRECTED BY THE AIRPORT.
2. CONTRACTOR SHALL MAINTAIN ALL FLASHERS IN WORKING ORDER FOR THE DURATION OF THE PROJECT.
3. LOW PROFILE BARRICADES SHALL BE PLACED END-TO-END OR INTERCONNECTED UNLESS OTHERWISE DIRECTED BY THE AIRPORT.



**2 FABRIC CLOSED RUNWAY MARKER**  
NOT TO SCALE

**CLOSED RUNWAY NOTES:**

1. FABRIC CLOSED RUNWAY MARKERS SHALL BE PLACED ON THE RUNWAY NUMERALS AT EACH END OF RUNWAY 16-34 DURING RUNWAY CLOSURE PERIODS.
2. FABRIC CLOSED RUNWAY MARKERS WILL BE SUPPLIED BY THE AIRPORT. CONTRACTOR IS RESPONSIBLE FOR PLACING, MAINTAINING, AND REMOVAL OF CLOSED RUNWAY MARKERS FOR EACH DAY OF CLOSURE.
3. CONTRACTOR SHALL PROVIDE MEANS TO SECURE THE FABRIC CLOSED RUNWAY MARKERS SO AS TO NOT BE AFFECTED BY WIND. THIS MAY CONSIST OF SANDBAGS, 2" X 6" LUMBER, OR COMBINATION THEREOF, OR AS OTHERWISE APPROVED BY THE RPR. SECURING MECHANISMS MUST BE YELLOW IN COLOR.



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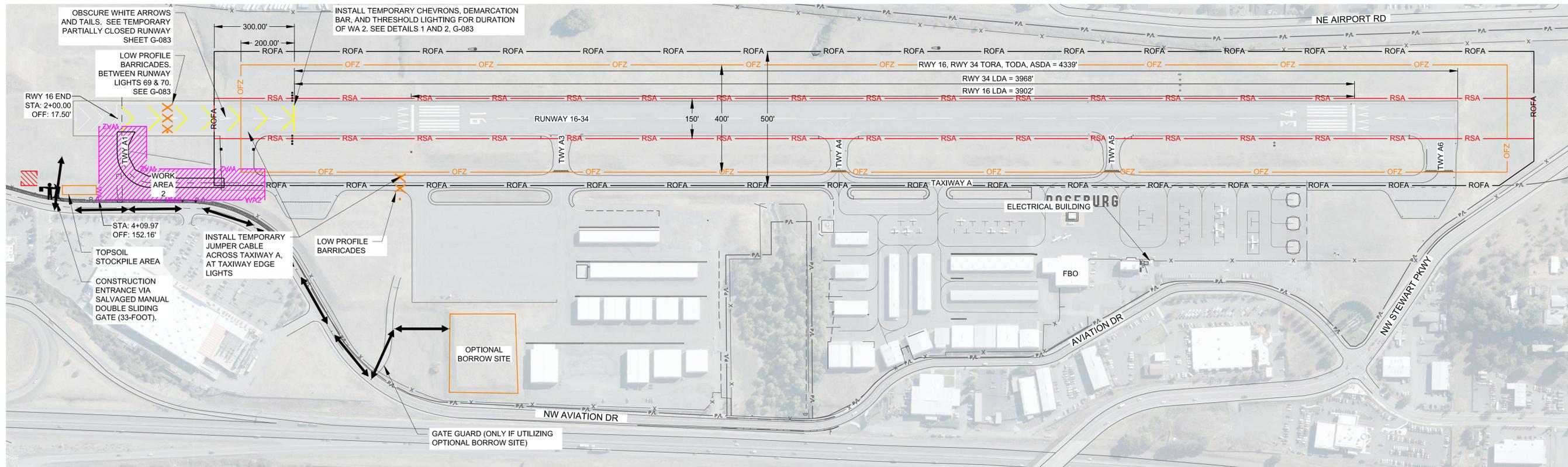
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SHEET CONTENTS  
 OPERATIONS &  
 PHASING PLAN -  
 WORK AREA 1

SHEET NO. 5 of 40



MAJOR WORK ELEMENTS	IMPACT TO AIRPORT OPERATIONS
 <p><b>WORK AREA 2</b></p> <p>-EXTEND TAXIWAY ALPHA 400' NORTH. CONSTRUCT CONNECTOR TAXIWAY A1. INSTALL RETAINING WALL, JET BLAST FENCE, PAVEMENT UNDERDRAINS, PAVEMENT MARKINGS, RUNWAY AND TAXIWAY EDGE LIGHTS, TAXIWAY GUIDANCE SIGNS.</p>	<p>TEMPORARY PARTIALLY CLOSED RUNWAY THAT WILL MODIFY DECLARED DISTANCES FOR RUNWAY 16/34 DURING CONSTRUCTION. WORK CONCURRENT WITH WORK AREA 1 LIMITED TO WORK OUTSIDE THE OBSTACLE FREE ZONE.</p>

**PROJECT WORK AREA REQUIREMENTS:**

- THIS WORK AREA REQUIRES A TEMPORARY PARTIALLY CLOSED RUNWAY AND IMPLEMENTATION OF MODIFIED DECLARED DISTANCES. CONVERSION TO THE PARTIALLY CLOSED RUNWAY CONFIGURATION SHALL BE COMPLETED IN ONE (1) CALENDAR DAY CONCURRENT WITH WORK AREA 1. WORK ELEMENTS NEEDED TO PARTIALLY CLOSE THE RUNWAY INCLUDE COMPLETION OF THE FOLLOWING:
  - OBSCURING THREE (3) RUNWAY ARROWS AND TAILS
  - INSTALLING SEVEN (7) CHEVRON MARKINGS
  - INSTALLING ONE (1) DEMARCATION BAR
  - RELOCATING EIGHT (8) EXISTING RUNWAY THRESHOLD LIGHT FIXTURES AND TRANSFORMERS AND INSTALLING THEM ON NEW BASE CANS LOCATED AT THE DEMARCATION BAR.
  - INSTALLING A TEMPORARY JUMPER BETWEEN RUNWAY LIGHTS R71 AND R72.
  - OBSCURING RUNWAY LIGHTS R67, R68, R69, & R70 LOCATED BETWEEN THE DEMARCATION BAR AND THE TEMPORARY JUMPER.
  - INSTALLING YELLOW/WHITE RUNWAY GLOBES ON RUNWAY LIGHTS R38, R39, R40, R41, AND R42.
- WORK AREA 2 SHALL BE SUBSTANTIALLY COMPLETED IN 75 CALENDAR DAYS. THE CONTRACTOR SHALL PROVIDE A SCHEDULE DURING THE PRE-CONSTRUCTION MEETING THAT IDENTIFIES THE INTENDED DATES AND SEQUENCE OF RUNWAY CLOSURE(S). START OF WORK AREA 2 MAY BE CONCURRENT WITH THE START OF WORK AREA 1.
- OPEN TRENCHES AND EXCAVATIONS ARE ALLOWED TO REMAIN OPEN WHILE THE RUNWAY IS OPERATIONAL PROVIDED THEY ARE LOCATED OUTSIDE THE ACTIVE RUNWAY SAFETY AREA AND TAXIWAY OBJECT FREE AREA. HOWEVER, MATERIAL STOCKPILES AND EQUIPMENT STORAGE IS PROHIBITED WITHIN THE RUNWAY OBJECT FREE AREA AT ALL TIMES WHEN THE RUNWAY IS OPEN.
- CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). CONTRACTOR REQUIRED TO SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) AS OUTLINED IN THE CSPP, AT THE PRE-CONSTRUCTION MEETING.
- DAILY SAFETY INSPECTION SHALL BE PERFORMED BY THE CONTRACTOR AS IDENTIFIED IN THE CSPP.
- A PROJECT SUPERINTENDENT OR DESIGNATED PROJECT REPRESENTATIVE SHALL BE ON-SITE AT ALL TIMES WHILE CREWS ARE IN THE FIELD. THIS INCLUDES A PRIME CONTRACTOR REPRESENTATIVE DURING ALL WORK BEING PERFORMED BY SUBCONTRACTORS.
- ALL COMPONENTS OF THE TAXIWAY A ELECTRICAL SYSTEM OUTSIDE OF THE PROJECT WORK AREA AND NOT OTHERWISE NOTED SHALL REMAIN OPERATIONAL.
- EQUIPMENT HEIGHT SHALL NOT EXCEED 25-FEET, UNLESS OTHERWISE APPROVED BY THE CITY.

**LEGEND:**

	CONTRACTOR ACCESS ROUTE
	RUNWAY SAFETY AREA
	RUNWAY OBJECT FREE AREA
	OBSTACLE FREE ZONE
	CONTRACTOR STAGING AREA
	CONSTRUCTION FLAGGER
	LOW-PROFILE BARRICADES

**RUNWAY 16-34 DECLARED DISTANCES**  
EFFECTIVE FOR THE DURATION OF WORK AREA 2.

	RUNWAY 16	RUNWAY 34
TORA	4339'	4339'
TODA	4339'	4339'
ASDA	4339'	4339'
LDA	3902'	3968'



**Mead & Hunt**  
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**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID



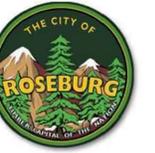
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SHEET CONTENTS  
OPERATIONS & PHASING PLAN - WORK AREA 2

SHEET NO. 6 of 40

**G-082**

X:\1821200\221217\_01\TECH\CAD\G-083 OPERATIONS SAFETY & PHASING PLAN - NOTES & DETAILS.DWG 4/17/2024 6:15:43 PM



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**ROSEBURG REGIONAL AIRPORT  
TAXIWAY A EXTENSION**  
3896 NW STEWART PARKWAY  
ROSEBURG, OR 97470

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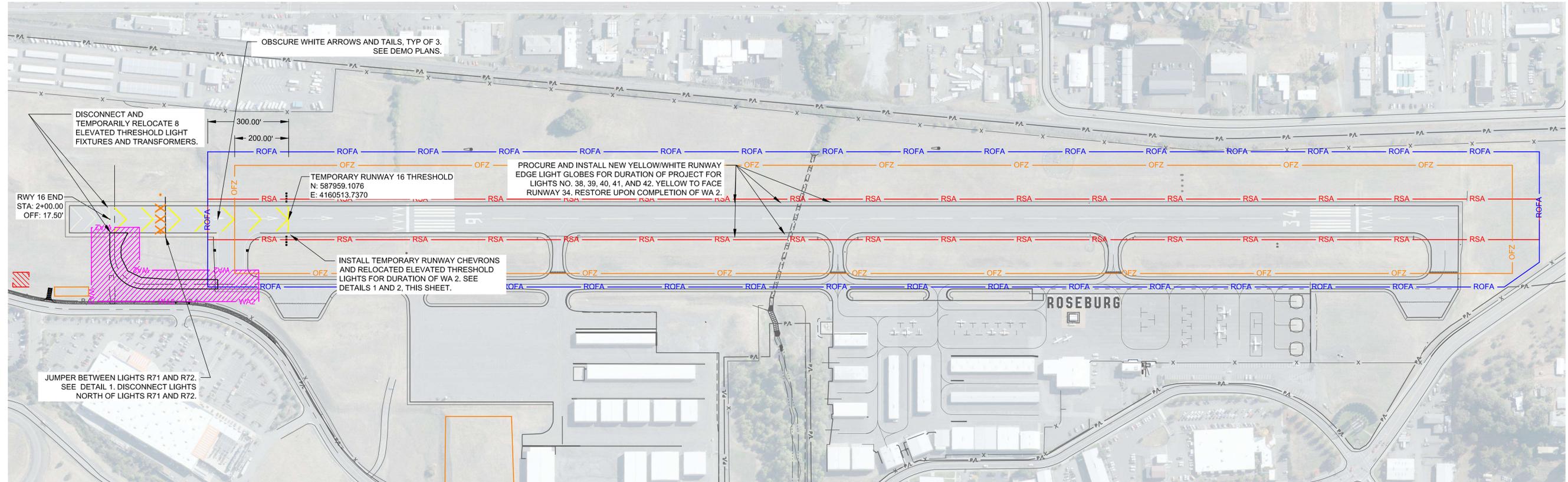


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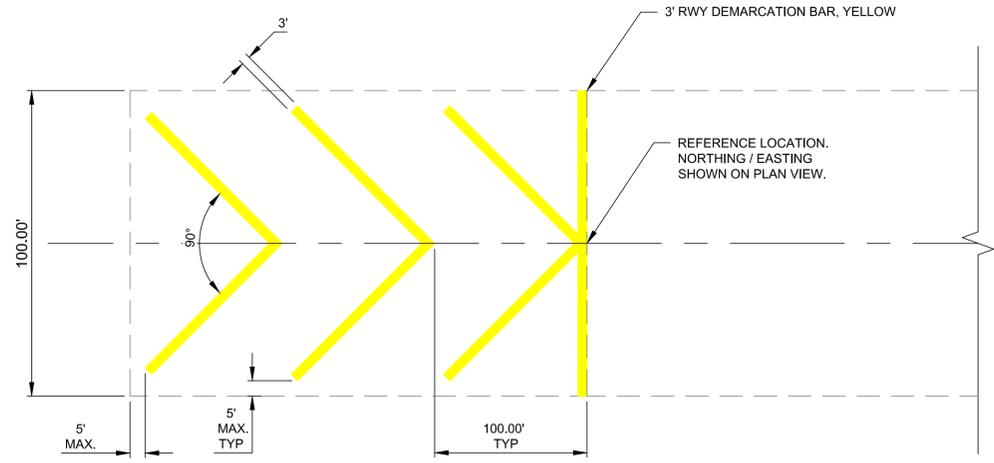
SHEET CONTENTS  
TEMPORARY PARTIALLY CLOSED RUNWAY

SHEET NO. 7 of 40

**G-083**

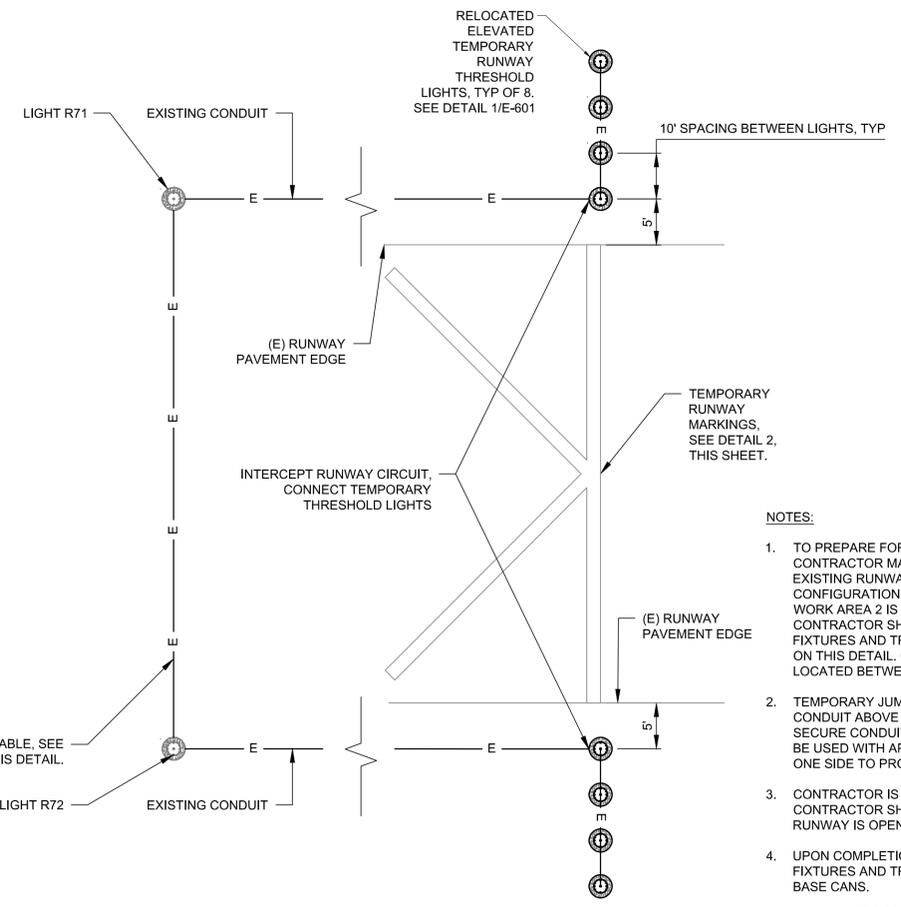


PAINTING OF THE TEMPORARY DEMARCATION BAR AND CHEVRONS SHALL OCCUR ON THE LAST CALENDAR DAY OF WORK AREA 1 DURING RUNWAY CLOSURE. WORK DESCRIBED ON THIS SHEET MUST BE COMPLETE BEFORE WORK INSIDE THE OBSTACLE FREE ZONE IN WORK AREA 2 MAY BE COMPLETED.



- NOTES:**
1. RUNWAY ARROWS AND TAILS WITHIN THE PARTIALLY CLOSED RUNWAY MUST BE OBSCURED PRIOR TO OPENING THE RUNWAY. SEE DEMO PLANS.
  2. TEMPORARY PARTIALLY CLOSED RUNWAY MARKINGS SHALL BE INSTALLED DURING THE ONE CALENDAR DAY CONVERSION. TEMPORARY MARKINGS SHALL NOT RECEIVE GLASS BEADS AND SHALL BE PAID UNDER ITEM P-620.
  3. CONTRACTOR TO RESTORE ORIGINAL MARKING CONFIGURATION UPON COMPLETION OF WA 2. TEMPORARY MARKINGS SHALL BE REMOVED IN ACCORDANCE WITH P-101. RUNWAY ARROWS AND TAILS SHALL BE REMARKED AND INCLUDE GLASS BEADS AND SHALL BE PAID UNDER ITEM P-620.

**2** TEMPORARY PARTIALLY CLOSED RUNWAY MARKINGS  
NOT TO SCALE



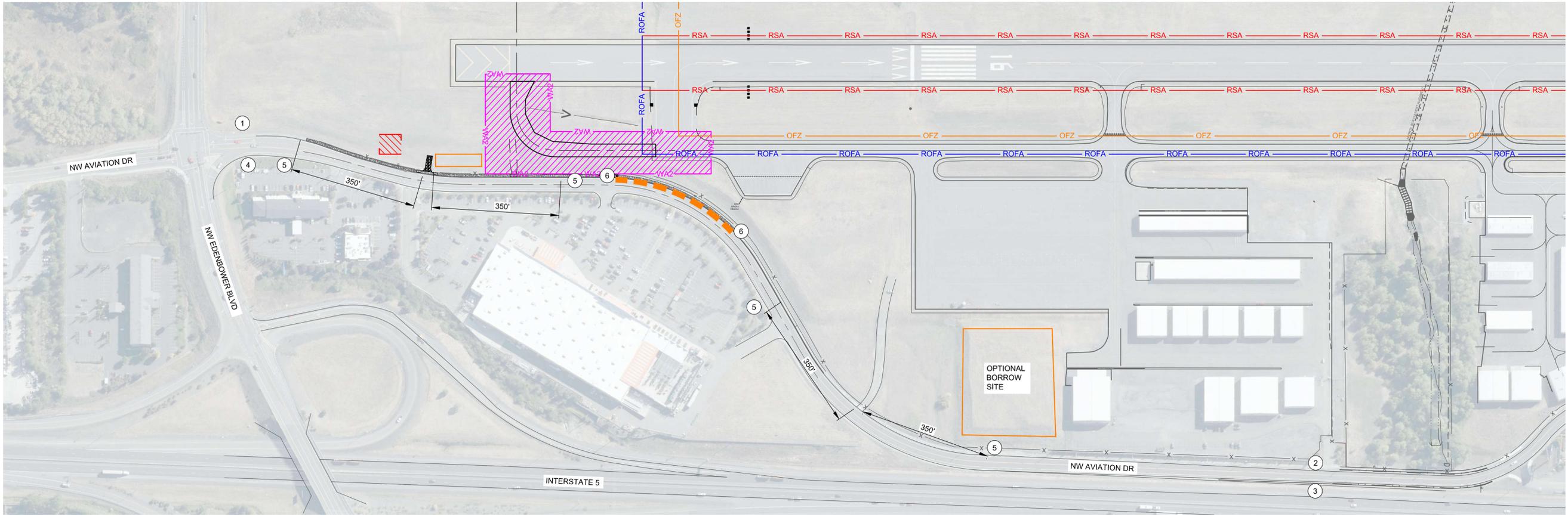
- NOTES:**
1. TO PREPARE FOR THE PARTIAL RUNWAY CLOSURE REQUIRED FOR WORK AREA 2, CONTRACTOR MAY INSTALL TEMPORARY THRESHOLD LIGHT BASE CANS DURING WORK AREA 1. EXISTING RUNWAY EDGE LIGHTING SHALL REMAIN OPERATIONAL IN THE EXISTING CONFIGURATION UNTIL THE ONE CALENDAR DAY RUNWAY CONVERSION DESCRIBED UNDER WORK AREA 2 IS PERFORMED. DURING THE ONE CALENDAR DAY CONVERSION, THE CONTRACTOR SHALL RELOCATE THE EIGHT (8) EXISTING RUNWAY THRESHOLD LIGHT FIXTURES AND TRANSFORMERS (AT STA. 2+00) AND INSTALL ON THE NEW BASE CANS SHOWN ON THIS DETAIL. CONTRACTOR SHALL OBSCURE RUNWAY LIGHTS R67, R68, R69, & R70 LOCATED BETWEEN THE DEMARCATION BAR AND THE TEMPORARY JUMPER.
  2. TEMPORARY JUMPER CABLE TO BE INSTALLED BETWEEN LIGHTS R71 AND R72 IN A 1" PVC CONDUIT ABOVE GROUND. TWO 2X8 PLANKS SECURED WITH SANDBAGS SHALL BE USED TO SECURE CONDUIT TO PAVEMENT TO PROTECT JUMPER FROM DAMAGE. OTHER METHODS MAY BE USED WITH APPROVAL BY THE RPR. LOW PROFILE BARRICADES SHALL BE INSTALLED ON ONE SIDE TO PROTECT ABOVE GROUND JUMPER.
  3. CONTRACTOR IS NOT ALLOWED TO LEAVE EXCAVATION OPEN WHEN THE RUNWAY IS OPEN. CONTRACTOR SHALL BACKFILL EXCAVATION WITH GRAVEL DURING PERIODS WHEN THE RUNWAY IS OPEN, OR OTHER METHOD APPROVED BY THE RPR PRIOR TO REOPENING RUNWAY.
  4. UPON COMPLETION OF WA 2, CONTRACTOR SHALL RELOCATE TEMPORARY THRESHOLD FIXTURES AND TRANSFORMERS BACK TO STA. 2+00 AND REMOVE TEMPORARY THRESHOLD BASE CANS.

**1** RUNWAY 16 TEMPORARY THRESHOLD DETAIL  
NOT TO SCALE

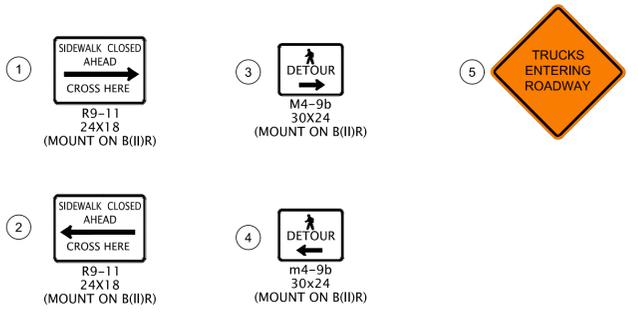
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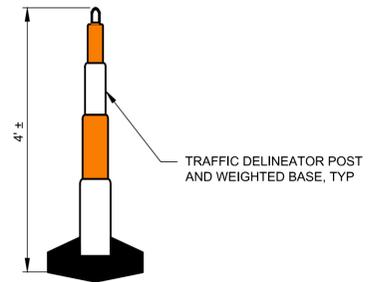
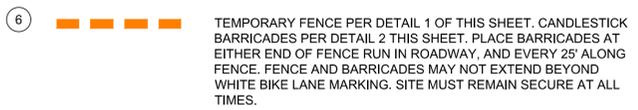
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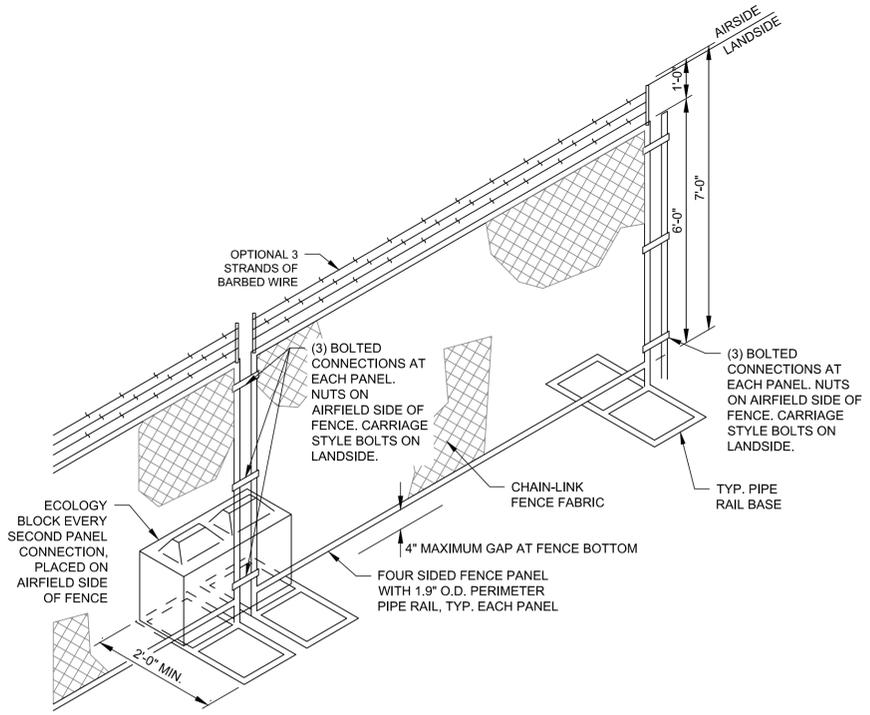
**KEYNOTES:**



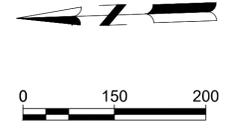
SIGNS TO MEET REQUIREMENTS OF STANDARD ODOT DETAIL TM844



**2 CANDLESTICK BARRICADE**  
 NO SCALE



**1 TEMPORARY SECURITY FENCE**  
 NO SCALE



**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

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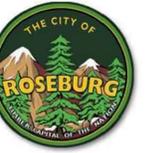


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SHEET CONTENTS  
**TRAFFIC CONTROL PLAN**

SHEET NO. 8 of 40

**G-084**



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

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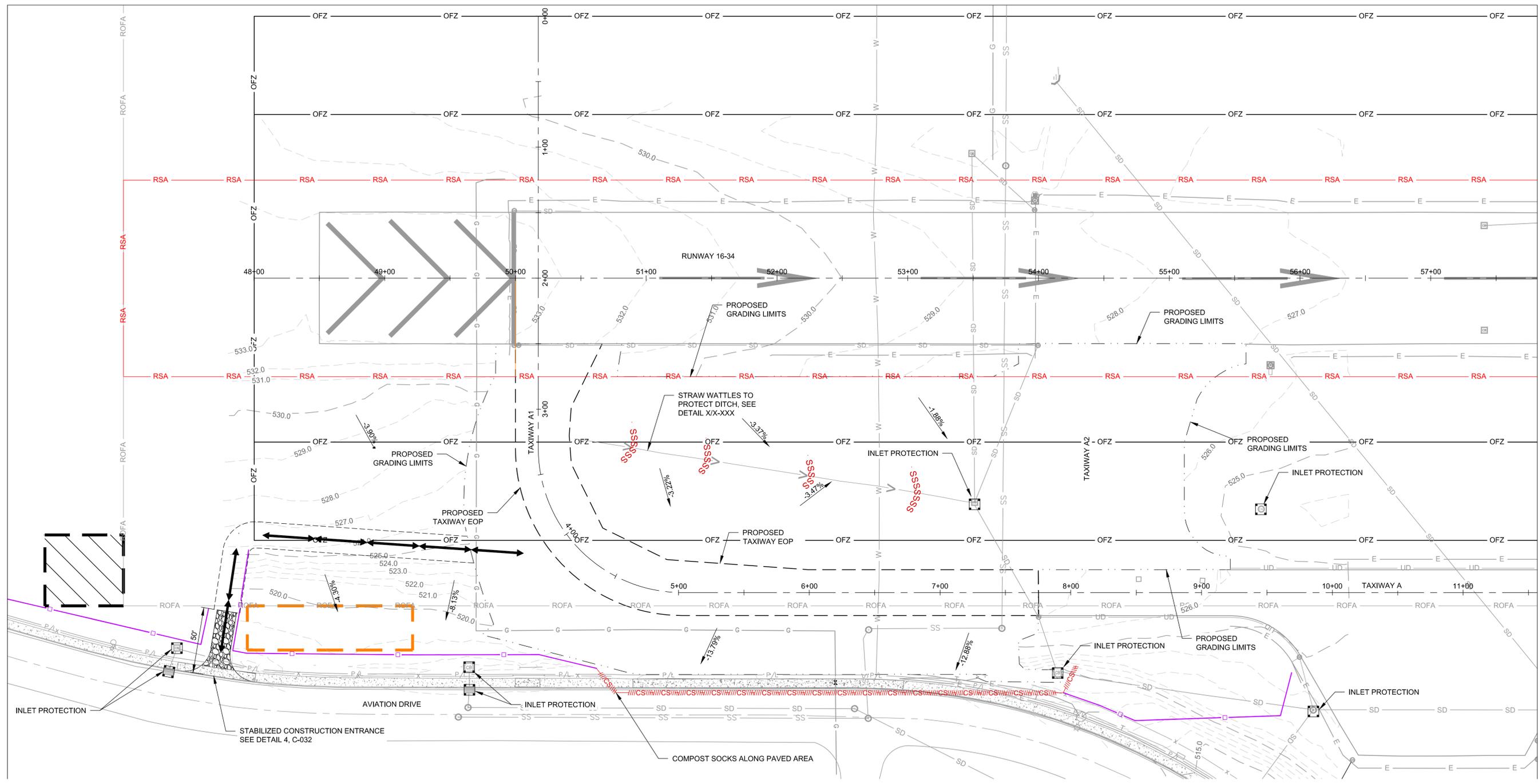


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SHEET CONTENTS  
INITIAL EROSION CONTROL PLAN

SHEET NO. 9 of 40

### C-021

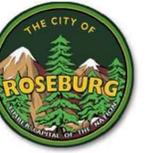


- LEGEND**
- INLET/CULVERT PROTECTION, SEE DETAIL 1/C-032
  - DIRECTION OF SURFACE FLOW
  - GRADING LIMITS
  - EXISTING CONTOUR LINES
  - CONTRACTOR HAUL ROUTE
  - STRAW WATTLES, SEE DETAIL 2/C-032
  - SEDIMENT FENCE, SEE DETAIL 2/C-033

- IMPLEMENTATION NOTES:**
1. SEE SHEET C-031 FOR EROSION AND SEDIMENT CONTROL 1200-C PERMIT REQUIREMENTS.
  2. THIS PLAN INDICATES THE MINIMUM EROSION CONTROL AND SEDIMENT MEASURES REQUIRED FOR THE PROJECT. THE CONTRACTOR SHALL EMPLOY ADDITIONAL BMPs, AS NECESSARY, TO KEEP SEDIMENT FROM LEAVING THE SITE OR FROM ENTERING STORM FACILITIES. ADDITIONAL BMPs WILL BE PAID THROUGH QUANTITY ADJUSTMENTS TO THE RESPECTIVE EROSION CONTROL BID ITEMS.
  3. AREAS USED FOR STAGING AND TEMPORARY HAUL ROUTES DURING CONSTRUCTION SHALL BE RESTORED TO THEIR ORIGINAL CONDITION.
  4. TEMPORARY HAUL ROUTES NEEDED FOR EXECUTION OF THE WORK WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. TEMPORARY HAUL ROUTES CONSTRUCTED BY THE CONTRACTOR SHALL BE REMOVED UPON COMPLETION OF HAULING OPERATIONS AND THE GROUND RESTORED TO ITS ORIGINAL GRADE, TOPSOILING, FERTILIZING, SEEDING, MULCHING, ETC.



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**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

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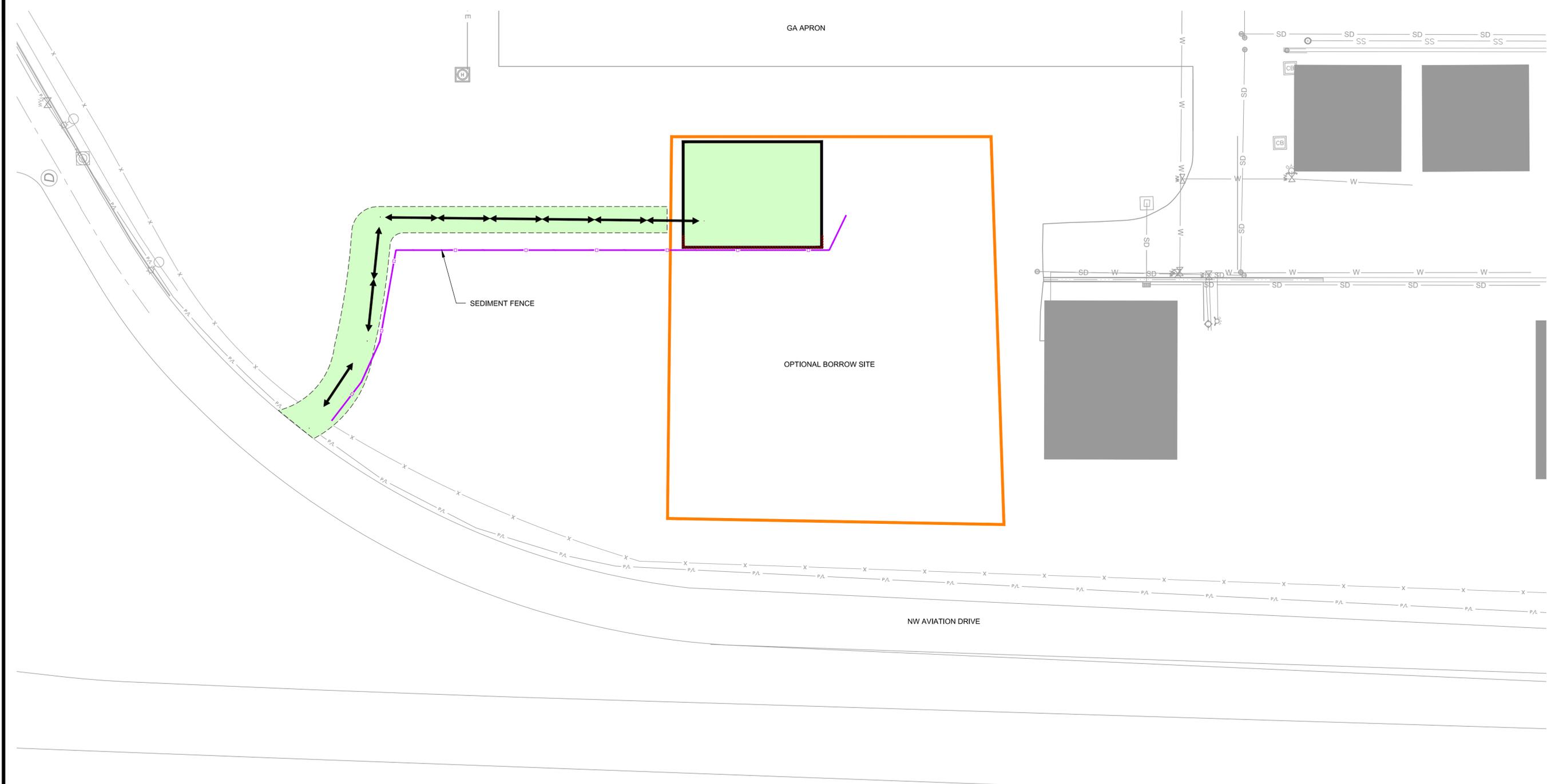


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SHEET CONTENTS  
EROSION CONTROL PLAN - BORROW SITE

SHEET NO. 10 of 40

## C-022



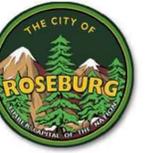
- LEGEND**
- TOPSOIL, SEED, & MULCH, SEE NOTES 2 AND 7
  - DIRECTION OF SURFACE FLOW
  - GRADING LIMITS
  - EXISTING CONTOUR LINES
  - CONTRACTOR HAUL ROUTE
  - SEDIMENT FENCE, SEE DETAIL 2/C-033

**IMPLEMENTATION NOTES:**

1. FINAL SITE STABILIZATION MEASURES INCLUDE RESTORING ALL DISTURBED AREAS OUTSIDE OF PAVED SURFACES WITH SEEDING PER SPECIFICATIONS T-901.
2. FINAL ACCEPTANCE CONTINGENT ON FINAL SITE STABILIZATION AS DESCRIBED IN SECTION 2.2.21 OF THE OREGON DEQ 1200C PERMIT.
3. IF CONTRACTOR ELECTS TO UTILIZE MATERIAL FROM THE OPTIONAL BORROW SITE, CONTRACTOR WILL BE RESPONSIBLE FOR REMOVING EXISTING TOPSOIL TO GAIN ACCESS TO SUITABLE MATERIAL. UPON COMPLETION OF BORROW EXCAVATION, CONTRACTOR SHALL SHAPE AND GRADE TO DRAIN (6:1 MAX SLOPE) PRIOR TO REPLACEMENT OF TOPSOIL, CONTRACTOR SHALL SEED AND MULCH DISTRIBUTED AREAS OF THE BORROW STOCKPILE. ALL WORK TO RESTORE THE BORROW EXCAVATION SITE AS DESCRIBED ABOVE SHALL BE INCIDENTAL TO THE BORROW EXCAVATION BID ITEM.
4. THE OPTIONAL BORROW SITE CONSISTS OF MATERIAL GENERATED BY REMOVAL OF THE SEDIMENTARY ROCK OUTCROP LOCATED ON THE NORTHEAST SIDE OF RUNWAY 16/34. THIS MATERIAL CONSISTS PRIMARILY OF PEBBLE TO COBBLE-SIZED CONGLOMERATE. FINE TO COARSE SANDSTONE AND MUDSTONE WAS ALSO OBSERVED IN THE STOCKPILE. THE ROCK RANGES FROM HIGHLY WEATHERED AND VERY WEAK (R1) TO SLIGHTLY WEATHERED TO FRESH AND MEDIUM HARD (R3). THIS MATERIAL IS HIGHLY SUSCEPTIBLE TO AIR AND MOISTURE SLAKING. IF UTILIZED, THE PROCESSING, IF NECESSARY, MAY INVOLVE CRUSHING, SCREENING, COMPACTING IN-PLACE, OR OTHER APPROVED METHODS TO ENSURE THE MAXIMUM PARTICLE SIZE DOES NOT EXCEED 2" AFTER FINAL PLACEMENT.
5. SEE SHEET C-031 FOR EROSION AND SEDIMENT CONTROL, 1200-C PERMIT REQUIREMENTS.
6. THIS PLAN INDICATES THE MINIMUM EROSION CONTROL AND SEDIMENT MEASURES REQUIRED FOR THE PROJECT. THE CONTRACTOR SHALL EMPLOY ADDITIONAL BMPs, AS NECESSARY, TO KEEP SEDIMENT FROM LEAVING THE SITE OR FROM ENTERING STORM FACILITIES. ADDITIONAL BMPs WILL BE PAID THROUGH QUANTITY ADJUSTMENTS TO THE RESPECTIVE EROSION CONTROL BID ITEMS.
7. AREAS USED FOR STAGING AND TEMPORARY HAUL ROUTES DURING CONSTRUCTION SHALL BE RESTORED TO THEIR ORIGINAL CONDITION.
8. TEMPORARY HAUL ROUTES NEEDED FOR EXECUTION OF THE WORK WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. TEMPORARY HAUL ROUTES CONSTRUCTED BY THE CONTRACTOR SHALL BE REMOVED UPON COMPLETION OF HAULING OPERATIONS AND THE GROUND RESTORED TO ITS ORIGINAL CONDITION. THIS WORK MAY INCLUDE, BUT NOT LIMITED TO, RESTORING THE AREA TO ORIGINAL GRADE, TOPSOILING, FERTILIZING, SEEDING, MULCHING, ETC.



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

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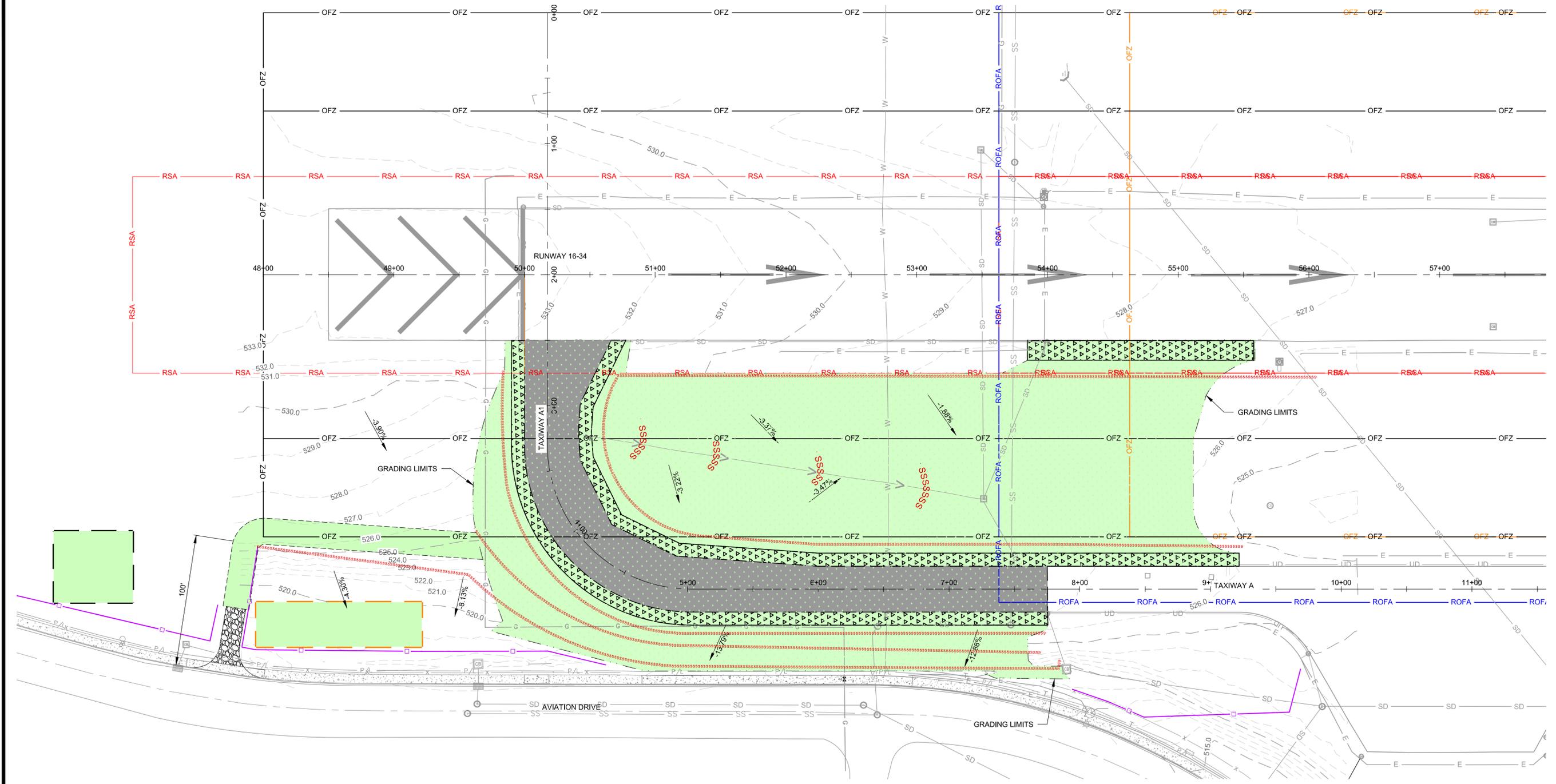


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DESIGNED BY: MLF  
DRAWN BY: MJK  
CHECKED BY: RJB  
DO NOT SCALE DRAWINGS

SHEET CONTENTS  
FINAL EROSION CONTROL PLAN

SHEET NO. 11 of 40

### C-023



#### LEGEND

- TOPSOIL, 9" DEPTH
- SEED & MULCH
- DIRECTION OF SURFACE FLOW
- SWALE
- GRADING LIMITS
- PROPOSED CONTOUR LINES
- EXISTING CONTOUR LINES
- CONTRACTOR HAUL ROUTE
- STRAW WATTLES, SEE DETAIL 2/C-032
- SILT FENCE, SEE DETAIL 2/C-033

#### IMPLEMENTATION NOTES:

1. REMOVE ALL TEMPORARY EROSION CONTROL MEASURES ONCE VEGETATION HAS BEEN ESTABLISHED.
2. CLEAN SEDIMENT FROM INLET PROTECTIONS AND/OR REPLACE SEDIMENT PROTECTION MEASURES THAT CANNOT BE RESTORED TO ORIGINAL CAPACITY.
3. FINAL SITE STABILIZATION MEASURES INCLUDE RESTORING ALL DISTURBED AREAS OUTSIDE OF PAVED SURFACES WITH SEEDING PER SPECIFICATIONS T-901.
4. WORK WITHIN THE RSA REQUIRES COORDINATION WITH THE AIRPORT AND RPR. SEE SHEET G-080 FOR MORE INFORMATION.
5. FINAL ACCEPTANCE CONTINGENT ON FINAL SITE STABILIZATION AS DESCRIBED IN SECTION 2.2.21 OF THE OREGON DEQ 1200C PERMIT.
6. SEE SHEET C-031 FOR EROSION AND SEDIMENT CONTROL 1200-C PERMIT REQUIREMENTS
7. THIS PLAN INDICATES THE MINIMUM EROSION CONTROL AND SEDIMENT MEASURES REQUIRED FOR THE PROJECT. THE CONTRACTOR SHALL EMPLOY ADDITIONAL BMPs, AS NECESSARY, TO KEEP SEDIMENT FROM LEAVING THE SITE OR FROM ENTERING STORM FACILITIES. ADDITIONAL BMPs WILL BE PAID THROUGH QUANTITY ADJUSTMENTS TO THE RESPECTIVE EROSION CONTROL BID ITEMS.
8. ALL STRIPPED TOPSOIL SHALL BE SPREAD OUTSIDE OF PAVED AREAS WITHIN THE GRADING LIMITS OF THE PROJECT SITE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ACCOUNT FOR THE TOPSOIL VOLUME WHEN IMPORTING SELECT BORROW.



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**STANDARD EROSION AND SEDIMENT CONTROL PLAN DRAWING NOTES:**

- ONCE KNOWN, INCLUDE A LIST OF ALL CONTRACTORS THAT WILL ENGAGE IN CONSTRUCTION ACTIVITIES ON SITE, AND THE AREAS OF THE SITE WHERE THE CONTRACTOR(S) WILL ENGAGE IN CONSTRUCTION ACTIVITIES. REVISE THE LIST AS APPROPRIATE UNTIL PERMIT COVERAGE IS TERMINATED (SECTION 4.4.c.i). IN ADDITION, INCLUDE A LIST OF ALL PERSONNEL (BY NAME AND POSITION) THAT ARE RESPONSIBLE FOR THE DESIGN, INSTALLATION, AND MAINTENANCE OF STORMWATER CONTROL MEASURES (E.G. ESCP DEVELOPER, BMP INSTALLER (SEE SECTION 4.10), AS WELL AS THEIR INDIVIDUAL RESPONSIBILITIES. (SECTION 4.4.c.ii)
- VISUAL MONITORING INSPECTION REPORTS MUST BE MADE IN ACCORDANCE WITH DEQ PERMIT REQUIREMENTS. (SECTION 6.5)
- INSPECTION LOGS MUST BE KEPT IN ACCORDANCE WITH DEQ'S 1200-C PERMIT REQUIREMENTS. (SECTION 6.5.4)
- RETAIN A COPY OF THE ESCP AND ALL REVISIONS ON SITE AND MAKE IT AVAILABLE ON REQUEST TO DEQ, AGENT, OR THE LOCAL MUNICIPALITY. (SECTION 4.7)
- THE PERMIT REGISTRANT MUST IMPLEMENT THE ESCP. FAILURE TO IMPLEMENT ANY OF THE CONTROL MEASURES OR PRACTICES DESCRIBED IN THE ESCP IS A VIOLATION OF THE PERMIT. (SECTIONS 4 AND 4.11)
- THE ESCP MUST BE ACCURATE AND REFLECT SITE CONDITIONS. (SECTION 4.8)
- SUBMISSION OF ALL ESCP REVISIONS IS NOT REQUIRED. SUBMITTAL OF THE ESCP REVISIONS IS ONLY UNDER SPECIFIC CONDITIONS. SUBMIT ALL NECESSARY REVISION TO DEQ OR AGENT WITHIN 10 DAYS. (SECTION 4.9)
- SEQUENCE CLEARING AND GRADING TO THE MAXIMUM EXTENT PRACTICAL TO PREVENT EXPOSED INACTIVE AREAS FROM BECOMING A SOURCE OF EROSION. (SECTION 2.2.2)
- CREATE SMOOTH SURFACES BETWEEN SOIL SURFACE AND EROSION AND SEDIMENT CONTROLS TO PREVENT STORMWATER FROM BYPASSING CONTROLS AND PONDING. (SECTION 2.2.3)
- IDENTIFY, MARK, AND PROTECT (BY CONSTRUCTION FENCING OR OTHER MEANS) CRITICAL RIPARIAN AREAS AND VEGETATION INCLUDING IMPORTANT TREES AND ASSOCIATED ROOTING ZONES, AND VEGETATION AREAS TO BE PRESERVED. IDENTIFY VEGETATIVE BUFFER ZONES BETWEEN THE SITE AND SENSITIVE AREAS (E.G. WETLANDS), AND OTHER AREAS TO BE PRESERVED, ESPECIALLY IN PERIMETER AREAS. (SECTION 2.2.1)
- PRESERVE EXISTING VEGETATION WHEN PRACTICAL AND RE-VEGETATE OPEN AREAS. RE-VEGETATE OPEN AREAS WHEN PRACTICABLE BEFORE AND AFTER GRADING OR CONSTRUCTION. IDENTIFY THE TYPE OF VEGETATIVE SEED MIX USED. (SECTION 2.2.5)
- MAINTAIN AND DELINEATE ANY EXISTING NATURAL BUFFER WITHIN THE 50-FEET OF WATERS OF THE STATE (SECTION 2.2.4)
- INSTALL PERIMETER SEDIMENT CONTROL, INCLUDING STORM DRAIN INLET PROTECTION AS WELL AS ALL SEDIMENT BASINS, TRAPS, AND BARRIERS PRIOR TO LAND DISTURBANCE. (SECTIONS 2.1.3)
- CONTROL BOTH PEAK FLOW RATES AND TOTAL STORMWATER VOLUME, TO MINIMIZE EROSION AT OUTLETS AND DOWNSTREAM CHANNELS AND STREAM BANKS. (SECTIONS 2.1.1 AND 2.2.16)
- CONTROL SEDIMENT AS NEEDED ALONG THE SITE PERIMETER AND AT ALL OPERATIONAL INTERNAL STORM DRAIN INLETS AT ALL TIMES DURING CONSTRUCTION, BOTH INTERNALLY AND AT THE SITE BOUNDARY. (SECTIONS 2.2.6 AND 2.2.13)
- ESTABLISH CONCRETE TRUCK AND OTHER EQUIPMENT WASHOUT AREAS BEFORE BEGINNING CONCRETE WORK. (SECTION 2.2.14)
- APPLY TEMPORARY AND/OR PERMANENT SOIL STABILIZATION MEASURES IMMEDIATELY ON ALL DISTURBED AREAS AS GRADING PROGRESSES. TEMPORARY OR PERMANENT STABILIZATIONS MEASURES ARE NOT REQUIRED FOR AREAS THAT ARE INTENDED TO BE LEFT UNVEGETATED, SUCH AS DIRT ACCESS ROADS OR UTILITY POLE PADS. (SECTIONS 2.2.20 AND 2.2.21)
- ESTABLISH MATERIAL AND WASTE STORAGE AREAS, AND OTHER NON-STORMWATER CONTROLS. (SECTION 2.3.7)
- KEEP WASTE CONTAINER LIDS CLOSED WHEN NOT IN USE AND CLOSE LIDS AT THE END OF THE BUSINESS DAY FOR THOSE CONTAINERS THAT ARE ACTIVELY USED THROUGHOUT THE DAY. FOR WASTE CONTAINERS THAT DO NOT HAVE LIDS, PROVIDE EITHER (1) COVER (E.G. A TARP, PLASTIC SHEETING, TEMPORARY ROOF) TO PREVENT EXPOSURE OF WASTES TO PRECIPITATION, OR (2) A SIMILARLY EFFECTIVE MEANS DESIGNED TO PREVENT THE DISCHARGE OF POLLUTANTS (E.G. SECONDARY CONTAINMENT). (SECTION 2.3.7)
- PREVENT TRACKING OF SEDIMENT ONTO PUBLIC OR PRIVATE ROADS USING BMP'S SUCH AS: CONSTRUCTION ENTRANCE, GRAVELED (OR PAVED) EXITS AND PARKING AREAS, GRAVEL ALL UNPAVED ROADS LOCATED ON-SITE, OR USE AN EXIT TIRE WASH. THESE BMP'S MUST BE IN PLACE PRIOR TO LAND-DISTURBING ACTIVITIES. (SECTION 2.2.7)
- WHEN TRUCKING SATURATED SOILS FROM THE SITE, EITHER USE WATER-TIGHT TRUCKS OR DRAIN LOADS ON SITE. (SECTION 2.2.7.f)
- CONTROL PROHIBITED DISCHARGES FROM LEAVING THE CONSTRUCTION SITE, I.E., CONCRETE WASH-OUT, WASTEWATER FROM CLEANOUT OF STUCCO, PAINT AND CURING COMPOUNDS. (SECTIONS 1.5 AND 2.3.9)
- ENSURE THAT STEEP SLOPE AREAS WHERE CONSTRUCTION ACTIVITIES ARE NOT OCCURRING ARE NOT DISTURBED. (SECTION 2.2.10)
- PREVENT SOIL COMPACTION IN AREAS WHERE POST-CONSTRUCTION INFILTRATION FACILITIES ARE TO BE INSTALLED. (SECTION 2.2.12)
- USE BMP'S TO PREVENT OR MINIMIZE STORMWATER EXPOSURE TO POLLUTANTS FROM SPILLS, VEHICLE AND EQUIPMENT FUELING, MAINTENANCE, AND STORAGE; OTHER CLEANING AND MAINTENANCE ACTIVITIES; AND WASTE HANDLING ACTIVITIES. THESE POLLUTANTS INCLUDE FUEL, HYDRAULIC FLUID, AND OTHER OILS FROM VEHICLES AND MACHINERY, AS WELL AS DEBRIS, FERTILIZER, PESTICIDES AND HERBICIDES, PAINTS, SOLVENTS, CURING COMPOUNDS AND ADHESIVES FROM CONSTRUCTION OPERATIONS. (SECTIONS 2.2.15 AND 2.3)
- PROVIDE PLANS FOR SEDIMENTATION BASINS THAT HAVE BEEN DESIGNED PER SECTION 2.2.17 AND STAMPED BY AN OREGON PROFESSIONAL ENGINEER. (SEE SECTION 2.2.17.a)
- IF ENGINEERED SOILS ARE USED ON SITE, A SEDIMENTATION BASIN/IMPONDMENT MUST BE INSTALLED. (SEE SECTIONS 2.2.17 AND 2.2.18)
- PROVIDE A DEWATERING PLAN FOR ACCUMULATED WATER FROM PRECIPITATION AND UNCONTAMINATED GROUNDWATER SEEPAGE DUE TO SHALLOW EXCAVATION ACTIVITIES. (SEE SECTION 2.4)
- IMPLEMENT THE FOLLOWING BMP'S WHEN APPLICABLE: WRITTEN SPILL PREVENTION AND RESPONSE PROCEDURES, EMPLOYEE TRAINING ON SPILL PREVENTION AND PROPER DISPOSAL PROCEDURES, SPILL KITS IN ALL VEHICLES, REGULAR MAINTENANCE SCHEDULE FOR VEHICLES AND MACHINERY, MATERIAL DELIVERY AND STORAGE CONTROLS, TRAINING AND SIGNAGE, AND COVERED STORAGE AREAS FOR WASTE AND SUPPLIES. (SECTION 2.3)
- USE WATER, SOIL-BINDING AGENT OR OTHER DUST CONTROL TECHNIQUE AS NEEDED TO AVOID WIND-BLOWN SOIL. (SECTION 2.2.9)
- THE APPLICATION RATE OF FERTILIZERS USED TO REESTABLISH VEGETATION MUST FOLLOW MANUFACTURER'S RECOMMENDATIONS TO MINIMIZE NUTRIENT RELEASES TO SURFACE WATERS. EXERCISE CAUTION WHEN USING TIME-RELEASE FERTILIZERS WITHIN ANY WATERWAY RIPARIAN ZONE. (SECTION 2.3.5)
- IF AN ACTIVE TREATMENT SYSTEM (FOR EXAMPLE, ELECTRO-COAGULATION, FLOCCULATION, FILTRATION, ETC.) FOR SEDIMENT OR OTHER POLLUTANT REMOVAL IS EMPLOYED, SUBMIT AN OPERATION AND MAINTENANCE PLAN (INCLUDING SYSTEM SCHEMATIC, LOCATION OF SYSTEM, LOCATION OF INLET, LOCATION OF DISCHARGE, DISCHARGE DISPERSION DEVICE DESIGN, AND A SAMPLING PLAN AND FREQUENCY) BEFORE OPERATING THE TREATMENT SYSTEM. OBTAIN ENVIRONMENTAL MANAGEMENT PLAN APPROVAL FROM DEQ BEFORE OPERATING THE TREATMENT SYSTEM. OPERATE AND MAINTAIN THE TREATMENT SYSTEM ACCORDING TO MANUFACTURER'S SPECIFICATIONS. (SECTION 1.2.9)
- TEMPORARILY STABILIZE SOILS AT THE END OF THE SHIFT BEFORE HOLIDAYS AND WEEKENDS, IF NEEDED. THE REGISTRANT IS RESPONSIBLE FOR ENSURING THAT SOILS ARE STABLE DURING RAIN EVENTS AT ALL TIMES OF THE YEAR. (SECTION 2.2)
- AS NEEDED BASED ON WEATHER CONDITIONS, AT THE END OF EACH WORKDAY SOIL STOCKPILES MUST BE STABILIZED OR COVERED, OR OTHER BMP'S MUST BE IMPLEMENTED TO PREVENT DISCHARGES TO SURFACE WATERS OR CONVEYANCE SYSTEMS LEADING TO SURFACE WATERS. (SECTION 2.2.8)
- SEDIMENT FENCE: REMOVE TRAPPED SEDIMENT BEFORE IT REACHES ONE THIRD OF THE ABOVE GROUND FENCE HEIGHT AND BEFORE FENCE REMOVAL. (SECTION 2.1.5.b)
- OTHER SEDIMENT BARRIERS (SUCH AS BIOBAGS); REMOVE SEDIMENT BEFORE IT REACHES TWO INCHES DEPTH ABOVE GROUND HEIGHT AND BEFORE BMP REMOVAL. (SECTION 2.1.5.c)
- CATCH BASINS: CLEAN BEFORE RETENTION CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT. SEDIMENT BASINS AND SEDIMENT TRAPS: REMOVE TRAPPED SEDIMENTS BEFORE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT AND AT COMPLETION OF PROJECT. (SECTION 2.1.5.d)
- WITHIN 24 HOURS, SIGNIFICANT SEDIMENT THAT HAS LEFT THE CONSTRUCTION SITE, MUST BE REMEDIATED. INVESTIGATE THE CAUSE OF THE SEDIMENT RELEASE AND IMPLEMENT STEPS TO PREVENT A RECURRENCE OF THE DISCHARGE WITHIN THE SAME 24 HOURS. ANY IN-STREAM CLEAN-UP OF SEDIMENT SHALL BE PERFORMED ACCORDING TO THE OREGON DEPARTMENT OF STATE LANDS REQUIRED TIMEFRAME. (SECTION 2.2.19.a)
- THE INTENTIONAL WASHING OF SEDIMENT INTO STORM SEWERS OR DRAINAGE WAYS MUST NOT OCCUR. VACUUMING OR DRY SWEEPING AND MATERIAL PICKUP MUST BE USED TO CLEANUP RELEASED SEDIMENTS. (SECTION 2.2.19)
- DOCUMENT ANY PORTION(S) OF THE SITE WHERE LAND DISTURBING ACTIVITIES HAVE PERMANENTLY CEASED OR WILL BE TEMPORARILY INACTIVE FOR 14 OR MORE CALENDAR DAYS. (SECTION 6.5.f)
- PROVIDE TEMPORARY STABILIZATION FOR THAT PORTION OF THE SITE WHERE CONSTRUCTION ACTIVITIES CEASE FOR 14 DAYS OR MORE WITH A COVERING OF BLOWN TACKIFIER, LOOSE STRAW, OR AN ADEQUATE COVERING OF COMPOST MULCH UNTIL WORK RESUMES ON THAT PORTION OF THE SITE. (SECTION 2.2.20)
- DO NOT REMOVE TEMPORARY SEDIMENT CONTROL PRACTICES UNTIL PERMANENT VEGETATION OR OTHER COVER OF EXPOSED AREAS IS ESTABLISHED. ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED, ALL TEMPORARY EROSION CONTROLS AND RETAINED MOVED AND DISPOSED OF PROPERLY, UNLESS NEEDED FOR LONG TERM USE FOLLOWING TERMINATION OF PERMIT COVERAGE. (SECTION 2.2.21)
- THE PERMITTEE IS REQUIRED TO MEET ALL THE CONDITIONS OF THE 1200C PERMIT. THIS ESCP AND GENERAL CONDITIONS HAVE BEEN DEVELOPED TO FACILITATE COMPLIANCE WITH THE 1200C PERMIT REQUIREMENTS. IN CASES OF DISCREPANCIES OR OMISSIONS, THE 1200C PERMIT REQUIREMENTS SUPERCEDE REQUIREMENTS OF THIS PLAN.

**BMP MATRIX FOR CONSTRUCTION PHASES**

REFER TO DEQ GUIDANCE MANUAL FOR A COMPREHENSIVE LIST OF AVAILABLE BMP'S.

	CLEARING	MASS GRADING	UTILITY INSTALLATION	PAVEMENT CONSTRUCTION	FINAL STABILIZATION
<b>EROSION PREVENTION</b>					
PRESERVE NATURAL VEGETATION	X**	X	X	X	X
GROUND COVER					X
HYDRAULIC APPLICATIONS					
PLASTIC SHEETING					
MATTING					
DUST CONTROL	X	X	X	X	X
TEMPORARY/ PERMANENT SEEDING			X	X	X
BUFFER ZONE	X**		X	X	X
BONDED FIBER MATRIX					
OTHER:					
<b>SEDIMENT CONTROL</b>					
SILT FENCE (PERIMETER)	X**	X		X	X
COMPOST FILTER SOCK (EXTERIOR)	X**	X	X	X	X
STRAW WATTLES	X**	X	X	X	X
FILTER BERM				X	X
INLET PROTECTION	X**	X	X	X	X
DEWATERING					
SEDIMENT TRAP					
OTHER:					
<b>RUN OFF CONTROL</b>					
CONSTRUCTION ENTRANCE	X**		X	X	X
PIPE SLOPE DRAIN					
OUTLET PROTECTION					
SURFACE ROUGHENING					
CHECK DAMS	X**				
OTHER:					
<b>POLLUTION PREVENTION</b>					
PROPER SIGNAGE	X	X	X	X	X
HAZ WASTE MGMT	X	X	X	X	X
SPILL KIT ON-SITE	X	X	X	X	X
CONCRETE WASHOUT AREA		X	X	X	X
OTHER:					

\*\* SIGNIFIES BMP THAT WILL BE INSTALLED PRIOR TO ANY GROUND DISTURBING ACTIVITY.

**RATIONALE STATEMENT**

A COMPREHENSIVE LIST OF AVAILABLE BEST MANAGEMENT PRACTICES (BMP) OPTIONS BASED ON DEQ'S GUIDANCE MANUAL HAS BEEN REVIEWED TO COMPLETE THIS EROSION AND SEDIMENT CONTROL PLAN. SOME OF THE ABOVE LISTED BMP'S WERE NOT CHOSEN BECAUSE THEY WERE DETERMINED TO NOT EFFECTIVELY MANAGE EROSION PREVENTION AND SEDIMENT CONTROL FOR THIS PROJECT BASED ON SPECIFIC SITE CONDITIONS, INCLUDING SOIL CONDITIONS TOPOGRAPHIC CONSTRAINTS, ACCESSIBILITY TO THE SITE, AND OTHER RELATED CONDITIONS, AS THE PROJECT PROGRESSES AND THERE IS A NEED TO REVISE THE ESC PLAN, AN ACTION PLAN WILL BE SUBMITTED.

INITIAL

PERMITTEE'S SITE INSPECTOR: \_\_\_\_\_

COMPANY/AGENCY: PHONE: \_\_\_\_\_  
 FAX: \_\_\_\_\_  
 E-MAIL: \_\_\_\_\_  
 DESCRIPTION OF EXPERIENCE: \_\_\_\_\_

DEVELOPER: **PLANNING / ENGINEERING /**

ROSEBURG REGIONAL AIRPORT MEAD & HUNT, INC  
 CONTACT: NIKKI MESSENGER CONTACT: MARK FORSLUND  
 900 SE DOUGLAS AVE. 9600 NE CASCADES PARKWAY, SUITE 100  
 ROSEBURG, OR 97470 PORTLAND, OR 97220  
 PHONE: 541-492-6730 PHONE: 971-256-0888

**ATTENTION EXCAVATORS:**

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**NARRATIVE DESCRIPTIONS:**

**EXISTING CONDITIONS:**

EXISTING CONDITIONS INCLUDE RUNWAYS, TAXIWAYS, APRONS, HANGARS, AND ASSOCIATED UTILITIES FOR AVIATION OPERATIONS.

**DEVELOPED CONDITIONS:**

BITUMINOUS PAVEMENT CONSTRUCTION OF TAXIWAY A AND ASSOCIATED DRAINAGE, LIGHTING, SIGNAGE, FENCING, RETAINING WALL, AND MARKINGS.

**SCHEDULE:**

- \* CLEARING: SEPTEMBER 2024 TO JUNE 2025
- \* MASS GRADING: SEPTEMBER 2024 TO JUNE 2025
- \* UTILITY INSTALLATION: SEPTEMBER 2024 TO JUNE 2025
- \* PAVEMENT CONSTRUCTION: SEPTEMBER 2024 TO JUNE 2025
- \* FINAL STABILIZATION: SEPTEMBER 2024 TO JUNE 2025

TOTAL SITE AREA = 120,192 SF = 2.76 ACRES

TOTAL DISTURBED AREA\*\* = 120,192 SF = 2.76 ACRES  
 \*\*DISTURBED AREA DOES NOT INCLUDE PAVEMENT MILL AND INLAY AREAS

**SITE SOIL CLASSIFICATION:**

SEVERAL HYDRIC SOIL TYPES ARE WITHIN THE PROJECT AREA. PRIMARILY THERE ARE MEDIUM STIFF, MEDIUM-BROWN SILTY CLAYS, WITH A COMPONENT OF FRACTURED COBBLE. UNDERNEATH THIS LAYER EXISTS A NATURAL STIFF, MEDIUM TO DARK BROWN, CLAY.

**RECEIVING WATER BODIES:**

NEAREST WATER BODY: NEWTON CREEK

**PROJECT LOCATION:**

PROJECT ADDRESS: 3896 NW STEWART PARKWAY,  
 ROSEBURG, OR 97470  
 ACTUAL ADDRESS: NORTH SIDE OF AIRFIELD,  
 ACCESS OFF OF AVIATION DRIVE  
 LATITUDE = 43° 14' 21.6102" N, LONGITUDE = 123° 21' 21.0569" W

**PROPERTY DESCRIPTION:**

REGIONAL AIRPORT

**INSPECTION FREQUENCY:**

SITE CONDITION	MINIMUM FREQUENCY
1. ACTIVE PERIOD	DAILY WHEN STORMWATER RUNOFF, INCLUDING RUNOFF FROM SNOW MELT, IS OCCURRING.  AT LEAST ONCE EVERY FOURTEEN (14) CALENDAR DAYS REGARDLESS OF WHETHER STORMWATER RUNOFF IS OCCURRING.
2. PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY.	ONCE TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING ORDER. ANY NECESSARY MAINTENANCE AND REPAIR MUST BE MADE PRIOR TO LEAVING THE SITE.
3. INACTIVE PERIODS GREATER THAN FOURTEEN (14) CONSECUTIVE CALENDAR DAYS.	ONCE EVERY MONTH.
4. PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER.	IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION.
5. PERIODS DURING WHICH DISCHARGE IS UNLIKELY DUE TO FROZEN CONDITIONS.	MONTHLY. RESUME MONITORING IMMEDIATELY UPON MELT, OR WHEN WEATHER CONDITIONS MAKE DISCHARGES LIKELY.

- \* HOLD A PRE-CONSTRUCTION MEETING OF PROJECT CONSTRUCTION PERSONNEL THAT INCLUDES THE INSPECTOR TO DISCUSS EROSION AND SEDIMENT CONTROL MEASURES AND CONSTRUCTION LIMITS. (Schedule A.8.c.i.(3))
- \* ALL INSPECTIONS MUST BE MADE IN ACCORDANCE WITH DEQ 1200-C PERMIT REQUIREMENTS.
- \* INSPECTION LOGS MUST BE KEPT IN ACCORDANCE WITH DEQ'S 1200-C PERMIT REQUIREMENTS.
- \* RETAIN A COPY OF THE ESCP AND ALL REVISIONS ON SITE AND MAKE IT AVAILABLE ON REQUEST TO DEQ, AGENT, OR THE LOCAL MUNICIPALITY. DURING INACTIVE PERIODS OF GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS, RETAIN THE ESCP AT THE CONSTRUCTION SITE OR AT ANOTHER LOCATION. (Schedule B.2.a)

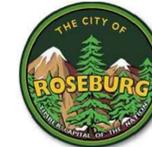


Know what's below.  
 Call before you dig.

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Mead and Hunt, Inc.  
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 Portland, OR 97220  
 phone: 503-548-1494  
 meadhunt.com



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ROSEBURG REGIONAL AIRPORT  
 TAXIWAY A EXTENSION  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID

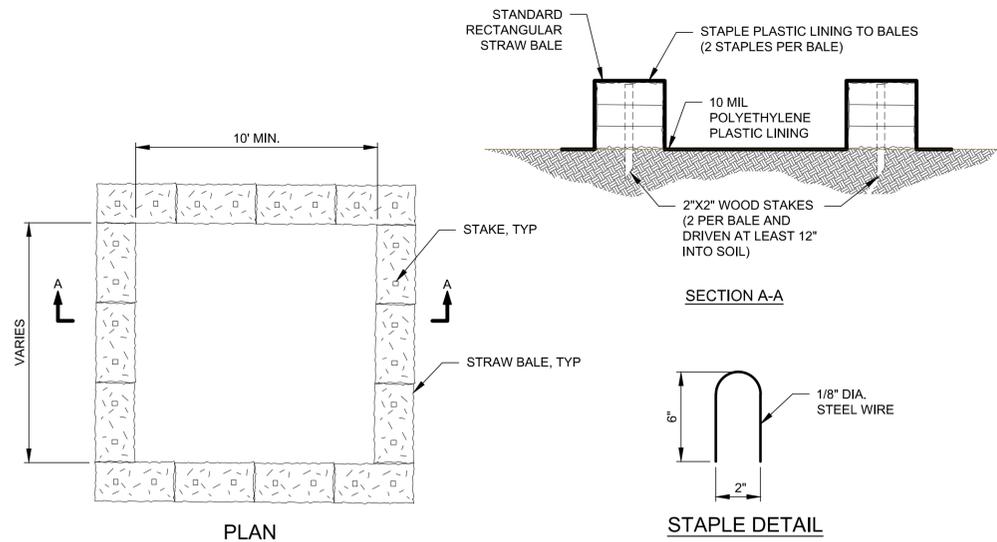


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SHEET CONTENTS  
 EROSION CONTROL NOTES

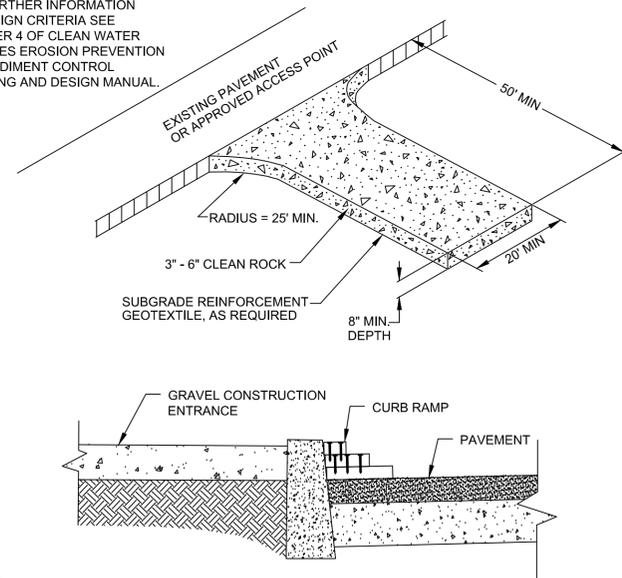
SHEET NO. 12 of 40

C-031



3 CONCRETE WASHOUT DETAIL  
NO SCALE

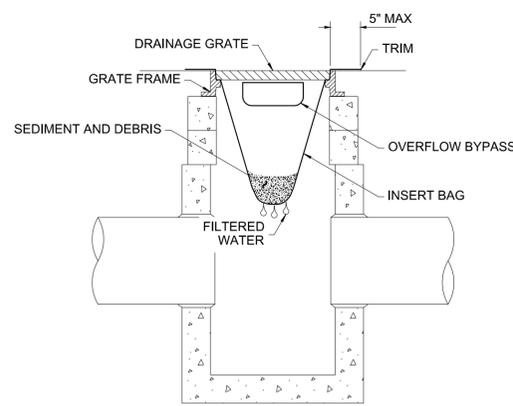
FOR FURTHER INFORMATION ON DESIGN CRITERIA SEE CHAPTER 4 OF CLEAN WATER SERVICES EROSION PREVENTION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL.



NOTES:

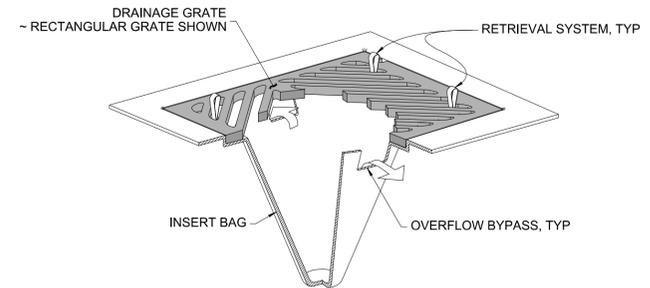
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAYS. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT.
2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.
4. WHERE RUNOFF CONTAINING SEDIMENT LADEN WATER IS LEAVING THE SITE VIA THE CONSTRUCTION ENTRANCE, OTHER MEASURES SHALL BE IMPLEMENTED TO DIVERT RUNOFF THROUGH AN APPROVED FILTERING SYSTEM.
5. DIMENSIONS  
MIN 50' LONG BY 20' WIDE 3-6" CLEAN ROCK.

4 STABILIZED CONSTRUCTION ENTRANCE  
NO SCALE

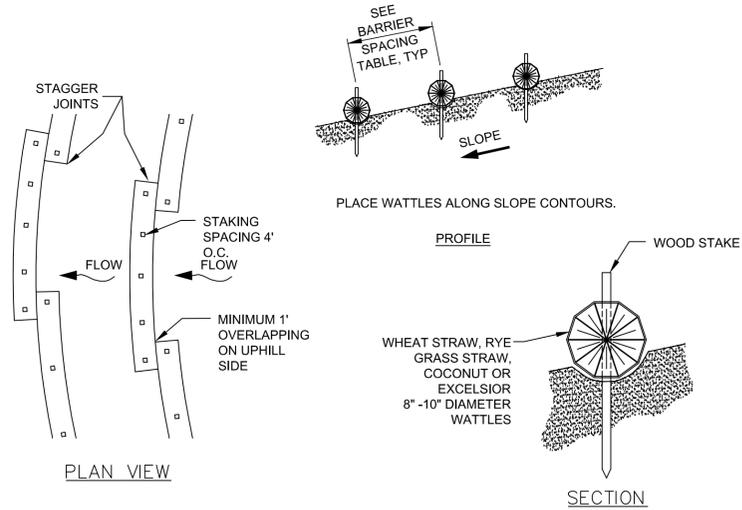


NOTES:

1. SIZE THE INSERT BAG FOR THE STORM WATER STRUCTURE IT WILL SERVICE.
2. THE INSERT BAG SHALL HAVE A BUILT-IN HIGH-FLOW RELIEF SYSTEM (OVERFLOW BYPASS).
3. THE RETRIEVAL SYSTEM MUST ALLOW REMOVAL OF THE INSERT BAG WITHOUT SPILLING THE COLLECTED MATERIAL.
4. PERFORM MAINTENANCE IN ACCORDANCE WITH ESCP NOTES AND 1200C PERMIT



1 INLET PROTECTION - INSERT BAG  
NO SCALE

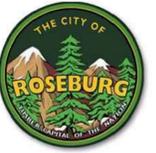


BARRIER SPACING		
% SLOPE	SLOPE H:V	MAXIMUM SPACING
10% OR FLATTER	10:1 OR FLATTER	300'
>10% OR <15%	>10:1 OR <7.5:1	150'
>15% OR <20%	>7.5:1 OR <5:1	100'
>20% OR <30%	>5:1 OR <3.5:1	50'
>30% OR <50%	>3.5:1 OR <2:1	25'

NOTES:

1. STAKING SPECIFICATIONS:
  - a. 1"x2" WOODEN STAKES
  - b. ADDITIONAL STAKES MAY BE INSTALLED ON DOWNHILL SIDE OF WATTLES, ON STEEP SLOPE OR HIGHLY EROSION SOILS.
2. SPACING IN ACCORDANCE WITH BARRIER SPACING TABLE.
3. REMOVE ALL ROCKS, CLODS, VEGETATION OR OTHER OBSTRUCTIONS SO THAT THE INSTALLED WATTLES WILL HAVE DIRECT CONTACT WITH THE SOIL.
4. INSTALL THE WATTLES IN A 2" DEEP TRENCH, INSURING THAT NO GAPS EXIST BETWEEN THE SOIL AND THE BOTTOM OF THE WATTLE. THE ENDS OF ADJACENT WATTLES SHALL BE OVERLAPPED 1 FT. MINIMUM TO PREVENT SEDIMENT PASSING THROUGH THE FIELD JOINT.

2 STRAW WATTLE DETAIL  
NO SCALE



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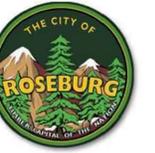


EXPIRES: 12 / 31 / 25

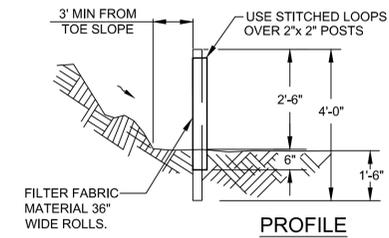
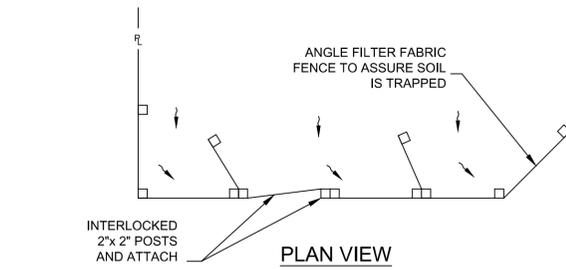
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MS# NO.: 1821200-221217.01  
DATE: APRIL 18, 2024  
DESIGNED BY: MLF  
DRAWN BY: MJK  
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SHEET CONTENTS  
EROSION CONTROL  
DETAILS

SHEET NO. 13 of 40

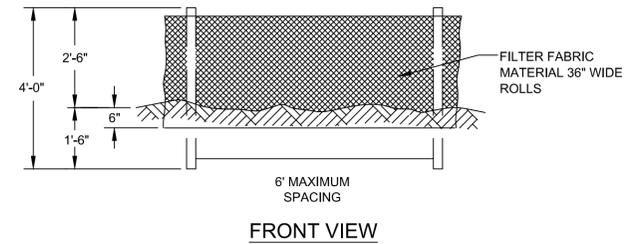


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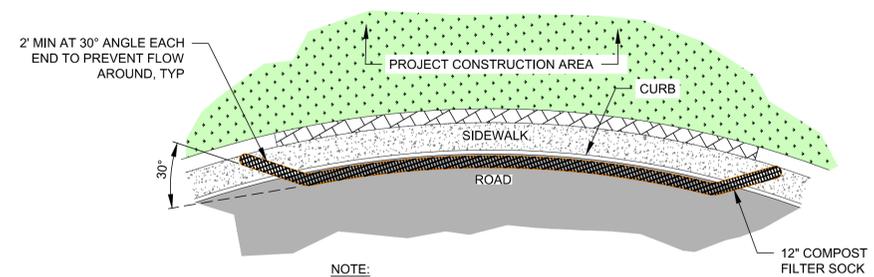


**NOTES:**

1. SEDIMENT FENCE TO HAVE STITCHED LOOPS AROUND 2" x 2" POSTS.
2. BURY BOTTOM OF FILTER FABRIC 6" VERTICALLY BELOW FINISHED GRADE.
3. 2" x 2" FIR, PINE OR STEEL FENCE POSTS.
4. POSTS TO BE INSTALLED ON UPHILL SIDE OF SLOPE.
5. COMPACT BOTH SIDES OF FILTER FABRIC TRENCH.
6. PANELS MUST BE PLACED ACCORDING TO SPACING ON PLANS.



**2** SEDIMENT FENCE DETAIL  
 NO SCALE



**NOTE:**

COMPOST SOCK SHALL BE 12" SYNTHETIC SOCK MESH FILLED WITH MIXED YARD DEBRIS COMPOST. COMPOST SOCKS SHALL BE PLACED AS A SEAL ON CONCRETE OR ASPHALT SURFACES. SEE EROSION CONTROL PLANS FOR LOCATION.

**1** COMPOST FILTER SOCK  
 NO SCALE

ROSEBURG REGIONAL AIRPORT  
 TAXIWAY A EXTENSION  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID

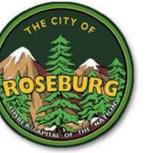


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SHEET CONTENTS  
 EROSION CONTROL  
 DETAILS

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**C-033**



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

ISSUED FOR BID



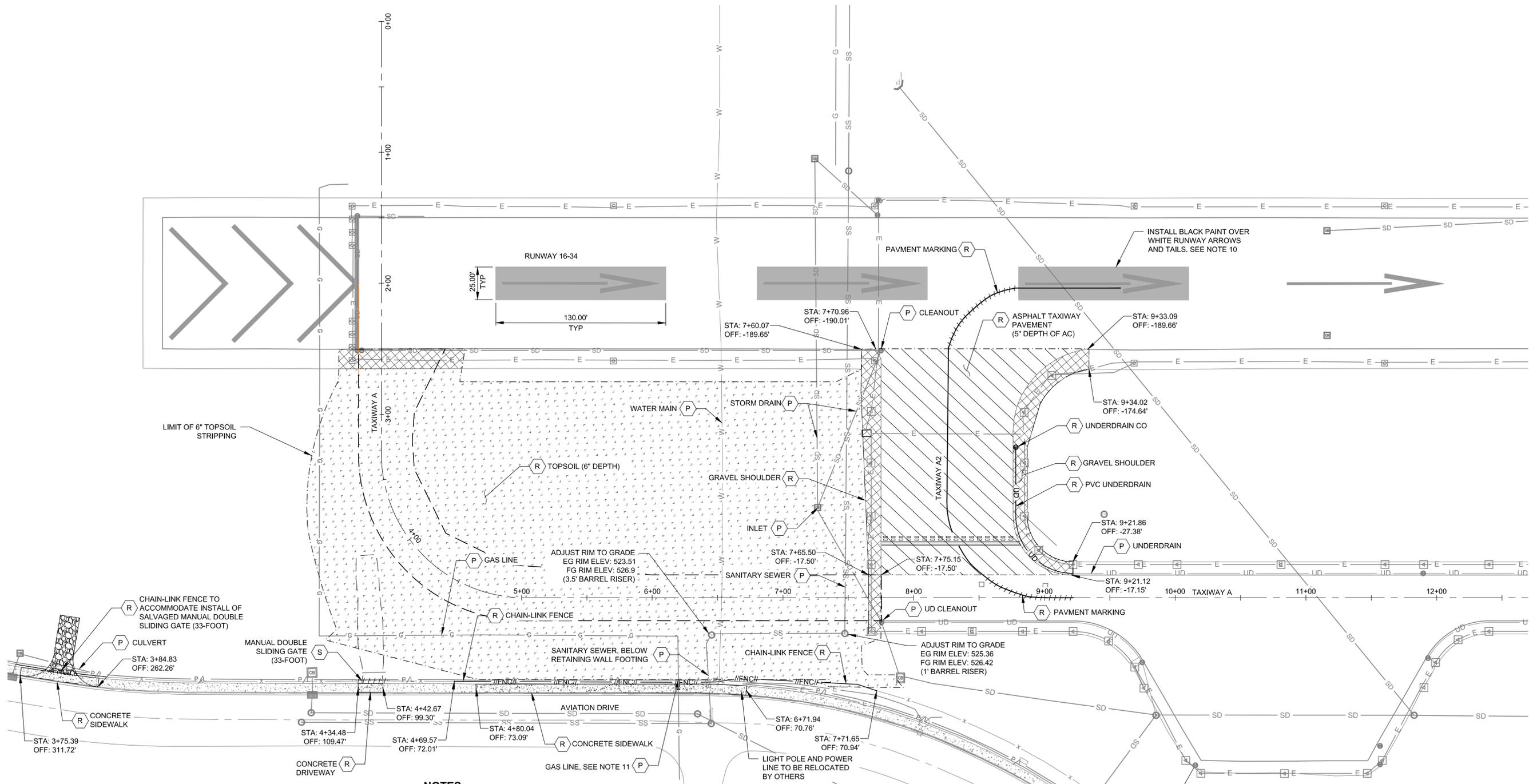
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SHEET CONTENTS  
DEMOLITION PLAN

SHEET NO. 15 of 40

### C-041



#### SHEET KEYNOTES:

- (A) ABANDON
- (R) REMOVE
- (P) PROTECT IN PLACE
- (S) SALVAGE

#### LEGEND:

- REMOVE TAXIWAY PAVEMENT
- REMOVE GRAVEL SHOULDER
- REMOVE 6" TOPSOIL
- REMOVE CONCRETE SIDEWALK
- COVER EXISTING PAVEMENT MARKING, SEE NOTE 10
- REMOVE TAXIWAY MARKING
- REMOVE CHAIN-LINK FENCE

#### NOTES:

1. THE STATION / OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE TAXIWAY A ALIGNMENT.
2. SEE SHEETS E-101 AND E-202 FOR ELECTRICAL DEMOLITION INCLUDING EDGE LIGHTS, SIGNS, CONDUIT, AND ELECTRICAL MANHOLES.
3. CONTRACTOR SHALL SURVEY ALONG THE MATCHLINE OF ALL EXISTING / PROPOSED PAVEMENTS IMMEDIATELY AFTER PAVEMENT REMOVAL (FOR ELEVATION VERIFICATION) AND SUPPLY THE RESULTS TO THE RPR. MATCHLINES DAMAGED BY THE CONTRACTOR AFTER REMOVAL SHALL BE SAWCUT TO PROVIDE A FLUSH TIE-IN.
4. CONTRACTOR TO MAINTAIN AIRPORT SECURITY AT ALL TIMES AS DEFINED IN THE CSPP. CONTRACTOR TO PROVIDE A WORK PLAN DEPICTING SEQUENCE OF EVENTS AND MAINTENANCE OF AIRPORT SECURITY PRIOR TO STARTING WORK.
5. THE EXISTING UTILITY INFORMATION SHOWN ON THESE DRAWINGS IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO DETERMINE THE TYPE AND LOCATION OF UTILITIES WITHIN THE PROJECT LIMITS AS NECESSARY TO AVOID DAMAGE OF EXISTING SYSTEMS. CONTRACTOR SHALL VERIFY LOCATIONS OF ALL UTILITIES NOT LATER THAN 2 WEEKS AFTER NTP. PROVIDE ANY DISCREPANCIES FROM PLAN UTILITIES SHOWN TO THE RPR FOR COORDINATION. NONDESTRUCTIVE POT HOLLING DURING THE PROJECT TO PROVIDE ADEQUATE PROTECTION OF THE UTILITIES DURING CONSTRUCTION IS THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL RECORD THE EXISTING UTILITY INFORMATION ON THE AS-BUILT DRAWINGS.
6. THE CONTRACTOR SHALL NOTIFY THE PROPER OWNER OF THEIR OPERATIONAL PLANS AFFECTING EACH UTILITY. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING ALL UTILITIES WITH THE RESPECTIVE RESPONSIBLE PARTIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RESPECTIVE COMPANY AND THE ENGINEER. ANY UTILITIES DISTURBED BY THE CONTRACTOR'S CARELESSNESS SHALL BE RESTORED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE OWNER AND ENGINEER.
7. THE CONTRACTOR WILL BE REQUIRED TO PROMPTLY REPAIR ANY PAVEMENT, PAVEMENT MARKING, LIGHTING, UTILITY, HAND HOLE, SIGNAGE, AND OTHER APPURTENANCES LOCATED OUTSIDE THE WORK AREA DAMAGED FROM CONSTRUCTION ACTIVITIES OR THE CONTRACTOR'S NEGLIGENCE AT NO ADDITIONAL COST TO THE AIRPORT.
8. PAVEMENT MARKINGS SHALL BE REMOVED IN ACCORDANCE WITH P-620-3.3.
9. VOIDS CREATED FROM REMOVALS SHALL BE BACKFILLED AND COMPACTED IN ACCORDANCE WITH ITEM P-101. UNLESS OTHERWISE SPECIFIED, ITEMS IDENTIFIED FOR REMOVAL SHALL BE DISPOSED OFF AIRPORT PROPERTY.
10. ARROWS AND TAILS TO BE COVERED BY BLACK PAINT IN THE SHAPE OF A BOX. EXTENTS OF MARKING REMOVAL MUST BE IN A 25' X 130' BOX. BLACK PAINT TO BE PAID FOR UNDER ITEM P-620.
11. THE OWNER HAS AUTHORIZED RELOCATION OF EXISTING GAS, POWER LINE, AND LIGHT POLE WITH THE PUBLIC UTILITIES. WORK WILL TAKE PLACE BY OTHERS ON THE PROJECT SITE. CONTRACTOR IS ADVISED THAT RELOCATION PATHWAY WILL BE WITHIN THE CONTRACTOR'S WORK SITE. COORDINATION AND COOPERATION WITH THE UTILITY COMPANIES SHALL BE IN ACCORDANCE WITH GP 70-05.
12. ADJUSTMENT OF SANITARY SEWER MANHOLES SHALL CONSIST OF INSTALLING A BARREL RISER ON EXISTING MANHOLES.
13. ONCE TOPSOIL HAS BEEN STRIPPED AND PRIOR EMBANKMENT OPERATIONS, THE CONTRACTOR SHALL SURVEY THE EXISTING GRADE (25' X 25' GRID) AND PROVIDE THE RESULTS TO THE RPR.
14. AT THE CONTRACTOR'S OPTION, 6-FOOT CHAIN-LINK FENCE MATERIALS REMOVED UNDER THIS PROJECT MAY BE SALVAGED AND REUSED TO CONSTRUCT 6-FOOT CHAIN-LINK FENCE CONTAINED UNDER OTHER ITEMS OF WORK. THESE MATERIALS ARE LIMITED TO CHAIN-LINK FENCE FABRIC, BARBED WIRE EXTENSION ARMS, TOP RAIL WITHOUT KINKS, AND GATE PANELS. SALVAGEABLE IS CONSIDERED FABRIC WITHOUT HOLES, TEARS, SNAGS, ETC.; EXTENSION ARMS WITHOUT KINKS; AND GATE PANELS THAT WHEN HUNG, PROVIDE THE FUNCTIONALITY OF A NEW GATE. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING ALL SALVAGED COMPONENTS ARE COMPATIBLE WITH THE NEW MATERIALS BEING PROVIDED.



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

ISSUED FOR BID

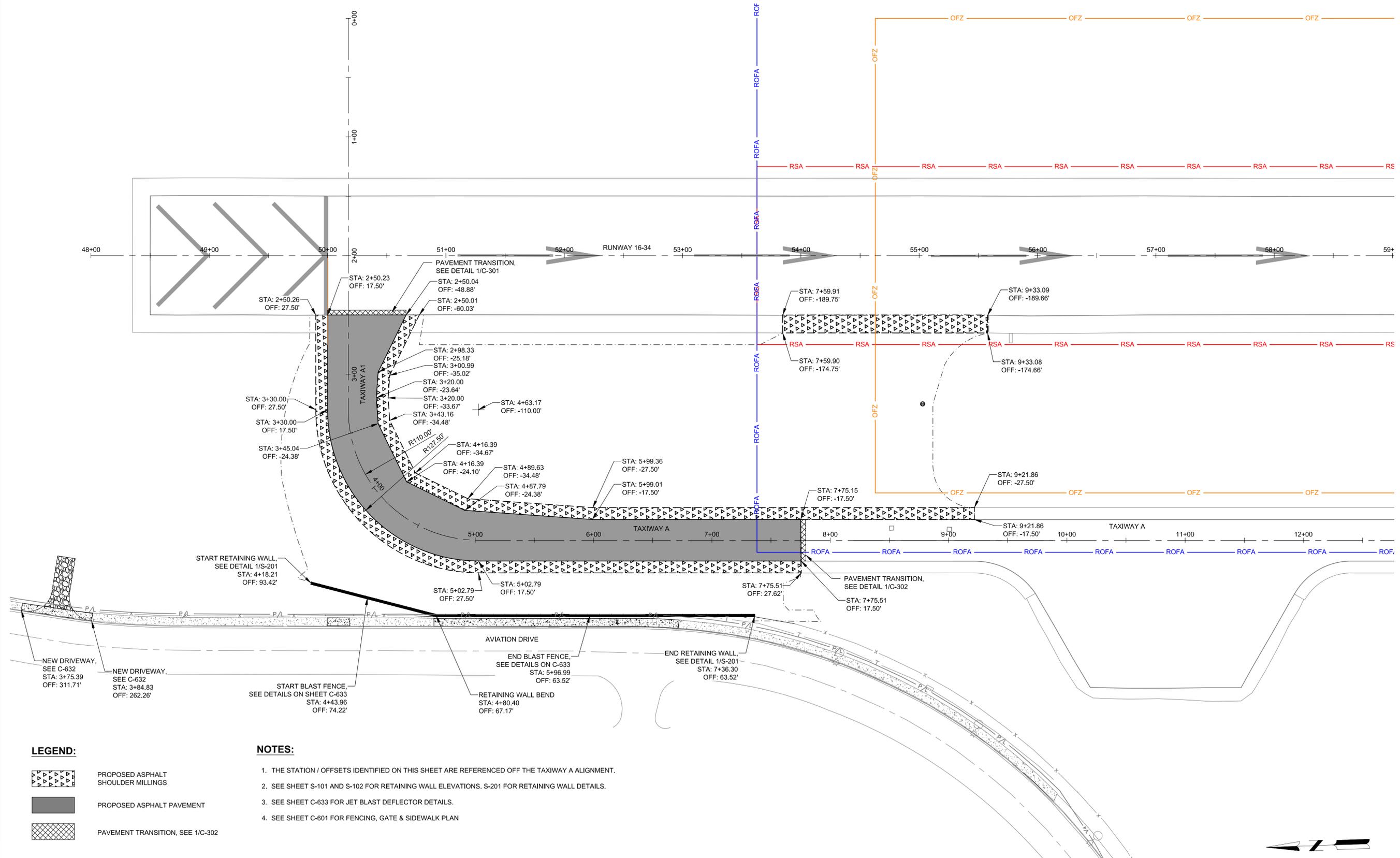


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DRAWN BY: MJK  
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SHEET CONTENTS  
SITE GEOMETRIC  
PLAN

SHEET NO. 16 of 40

### C-081

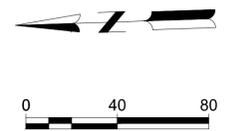


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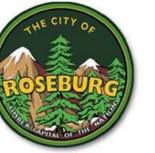
- PROPOSED ASPHALT SHOULDER MILLINGS
- PROPOSED ASPHALT PAVEMENT
- PAVEMENT TRANSITION, SEE 1/C-302

#### NOTES:

1. THE STATION / OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE TAXIWAY A ALIGNMENT.
2. SEE SHEET S-101 AND S-102 FOR RETAINING WALL ELEVATIONS. S-201 FOR RETAINING WALL DETAILS.
3. SEE SHEET C-633 FOR JET BLAST DEFLECTOR DETAILS.
4. SEE SHEET C-601 FOR FENCING, GATE & SIDEWALK PLAN



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

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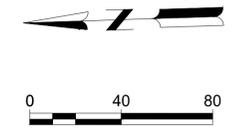
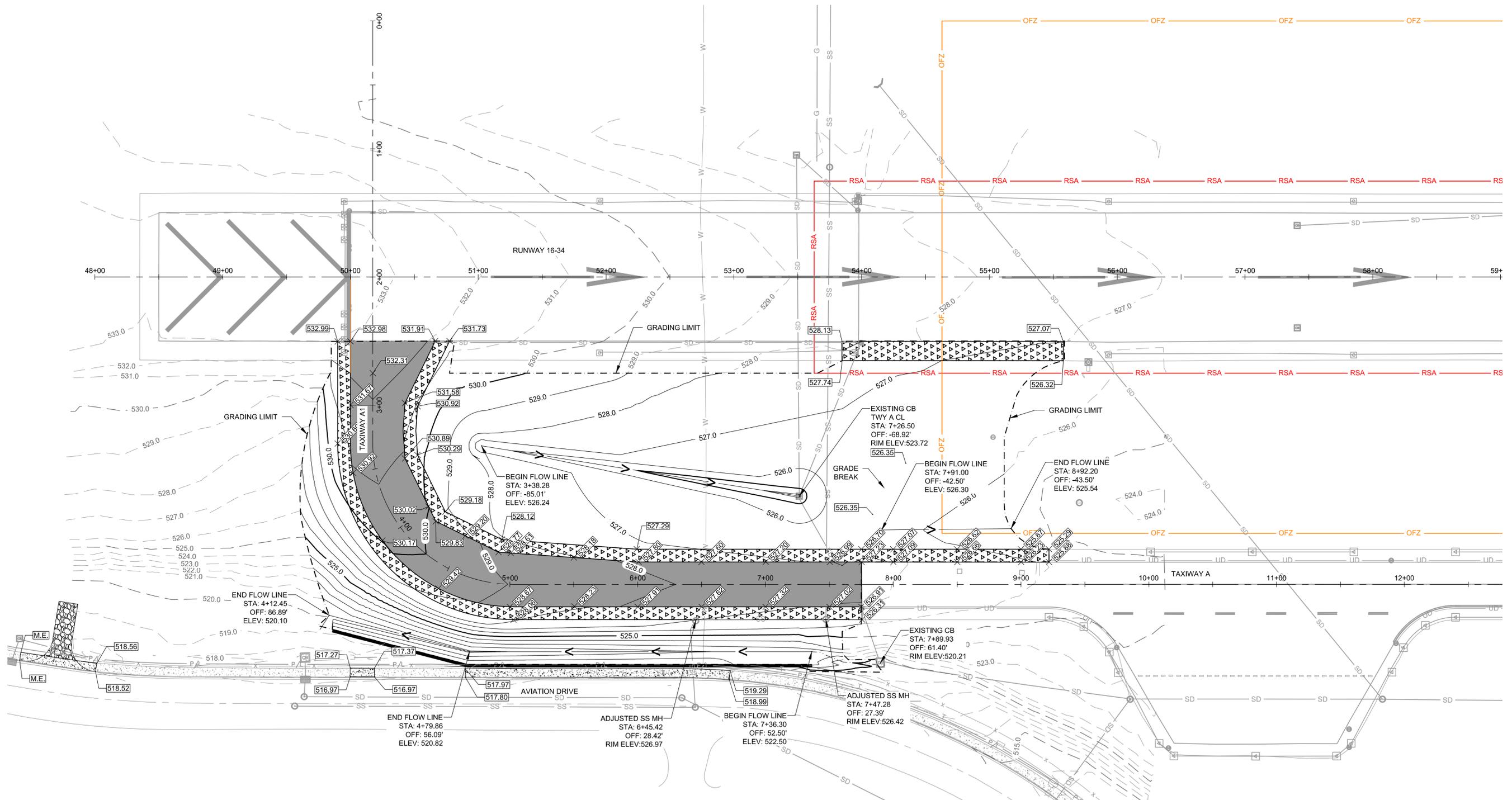


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SHEET CONTENTS  
GRADING & DRAINAGE PLAN

SHEET NO. 17 of 40

### C-101



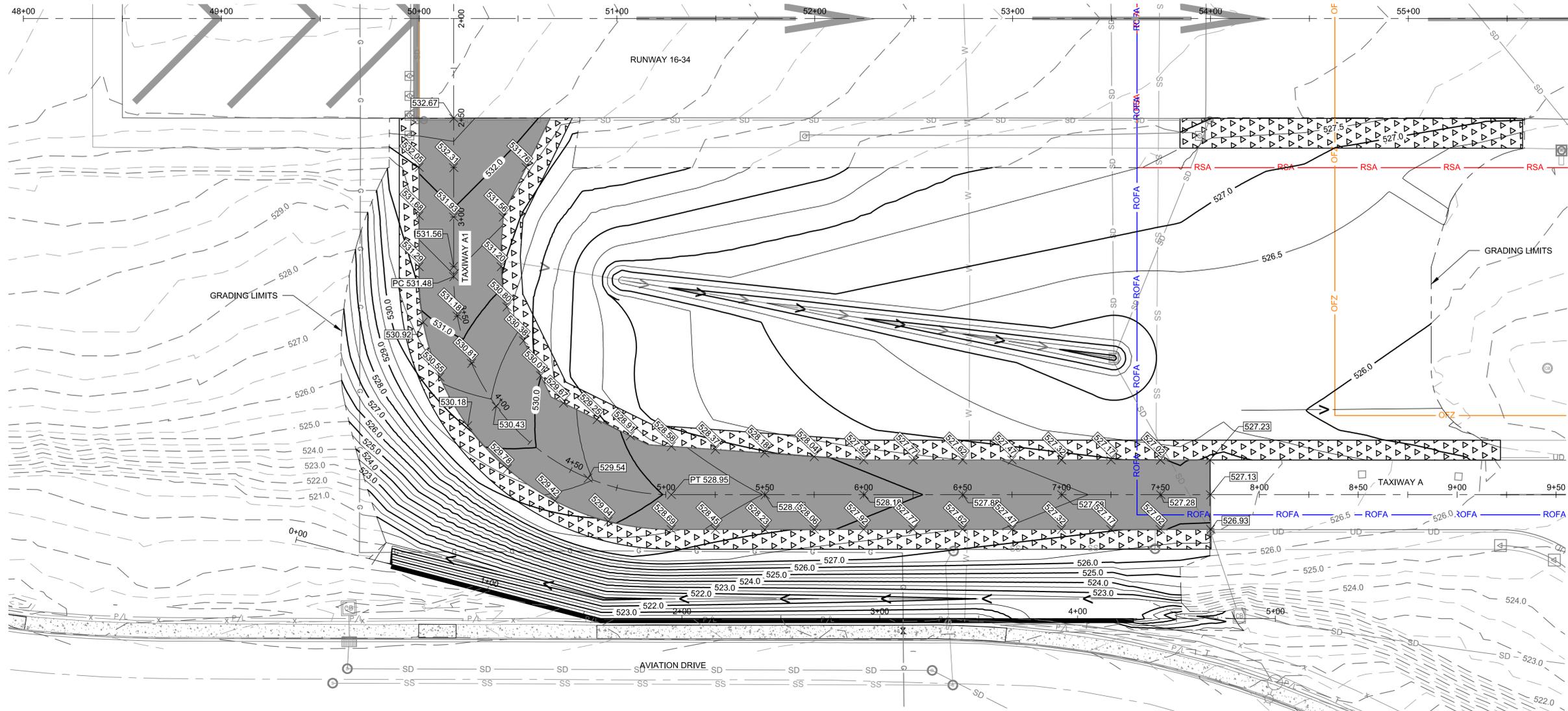
**LEGEND:**

- GRADING LIMIT
- PROPOSED ASPHALT PAVEMENT
- PROPOSED ASPHALT SHOULDER MILLINGS
- MATCH EXISTING GRADE

**GRADING NOTES:**

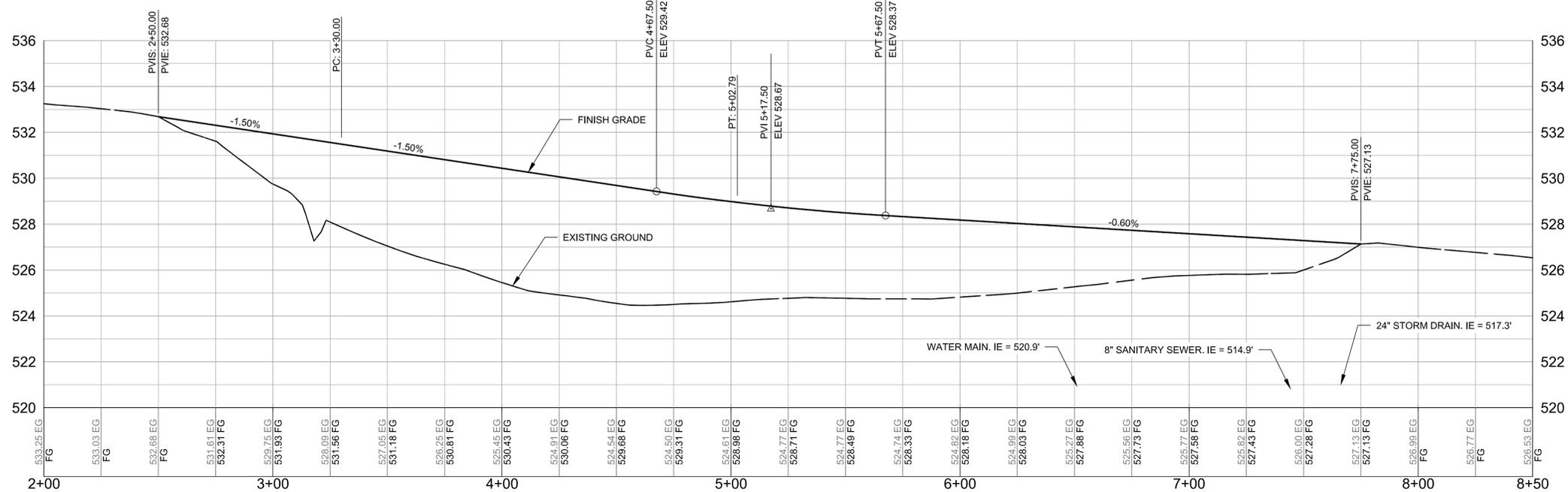
1. THE STATION / OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE EXISTING TAXIWAY A ALIGNMENT. SEE SHEET G-021 FOR ALIGNMENT DATA.
2. ELEVATIONS SHOWN ON TAXIWAY A ARE FINISHED GRADES ON TOP OF PAVEMENT. GRADES SHOWN OFF THE TAXIWAY ARE FINISHED GRADES OF TOPSOIL.
3. UNCLASSIFIED EXCAVATION PERFORMED UNDER AREAS TO BE PAVED (TO MEET THE TYPICAL SECTION DEPTH) SHALL BE UTILIZED AS EMBANKMENT MATERIAL OUTSIDE OF PAVED AREAS.
4. ALL STRIPPED TOPSOIL SHALL REMAIN ONSITE AND BE EVENLY DISTRIBUTED WITHIN THE GRADING LIMITS OUTSIDE OF PAVED AREAS. THE CONTRACTOR IS RESPONSIBLE FOR MAKING ADJUSTMENTS TO THE SELECT BORROW QUANTITY, AS NECESSARY, TO ACCOMMODATE THE STRIPPED VOLUME OF TOPSOIL. PRIOR TO PLACING TOPSOIL OVER THE SITE, THE CONTRACTOR SHALL SURVEY THE GRADE (25' X 25' GRID) AND PROVIDE THE RESULTS TO THE RPR.
5. THE OPTIONAL BORROW SITE CONSISTS OF MATERIAL GENERATED BY REMOVAL OF THE SEDIMENTARY ROCK OUTCROP LOCATED ON THE NORTHEAST SIDE OF RUNWAY 16/34. THIS MATERIAL CONSISTS PRIMARILY OF PEBBLE TO COBBLE-SIZED CONGLOMERATE. FINE TO COARSE SANDSTONE AND MUDSTONE WAS ALSO OBSERVED IN THE STOCKPILE. THE ROCK RANGES FROM HIGHLY WEATHERED AND VERY WEAK (R1) TO SLIGHTLY WEATHERED TO FRESH AND MEDIUM HARD (R3). THIS MATERIAL IS HIGHLY SUSCEPTIBLE TO AIR AND MOISTURE SLAKING. IF UTILIZED BY THE PROCESSING, IF NECESSARY, MAY INVOLVE CRUSHING, SCREENING, COMPACTING IN-PLACE, OR OTHER APPROVED METHODS TO ENSURE THE MAXIMUM PARTICLE SIZE DOES NOT EXCEED 2" AFTER FINAL PLACEMENT.

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NOTE: UTILITY CROSSING ELEVATIONS SHOWN AT TAXIWAY A CENTERLINE. UTILITY INFORMATION SHOWN ARE FROM RECORD DRAWINGS. CONTRACTOR TO VERIFY ACTUAL UTILITY DEPTHS, SIZES, AND LOCATION DURING CONSTRUCTION.

PVI STA=5+17.50  
 PVI ELEV=528.67  
 LOW PNT STA=5+67.50  
 LOW PNT ELEV=528.37  
 K=110.8  
 VC=100.00'



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**ROSEBURG REGIONAL AIRPORT  
 TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID

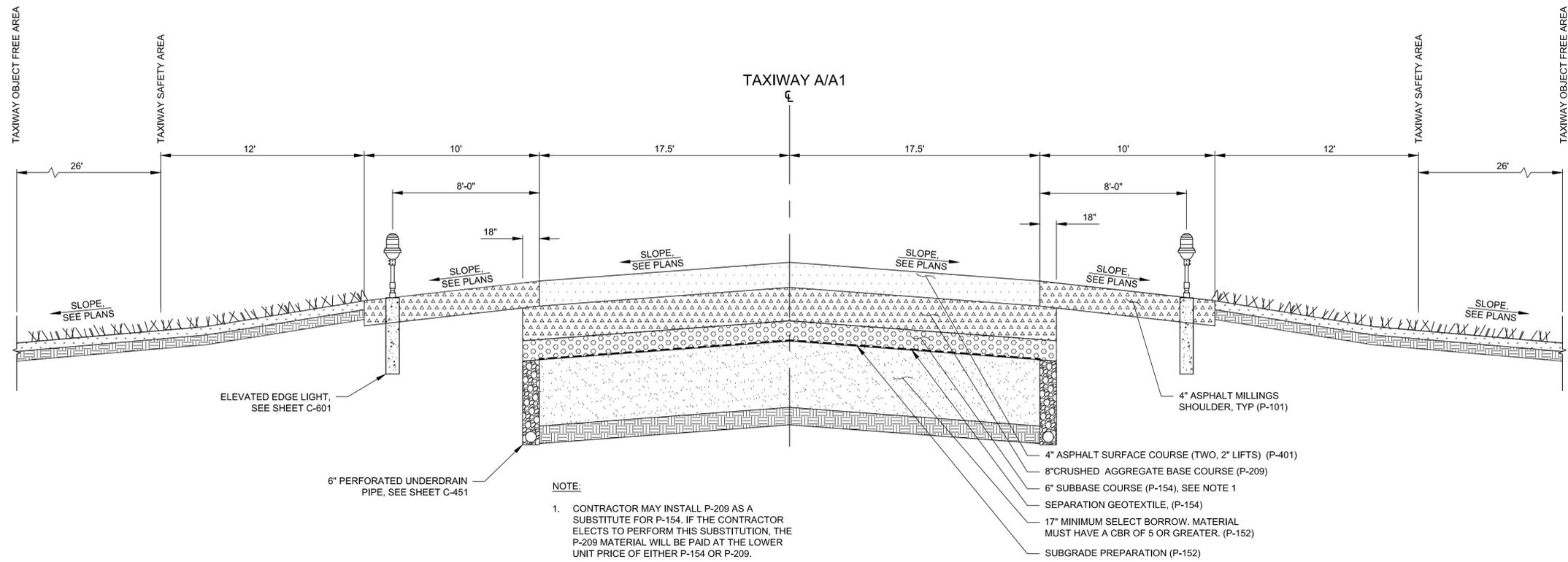
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SHEET CONTENTS  
**TAXIWAY A PLAN & PROFILE**

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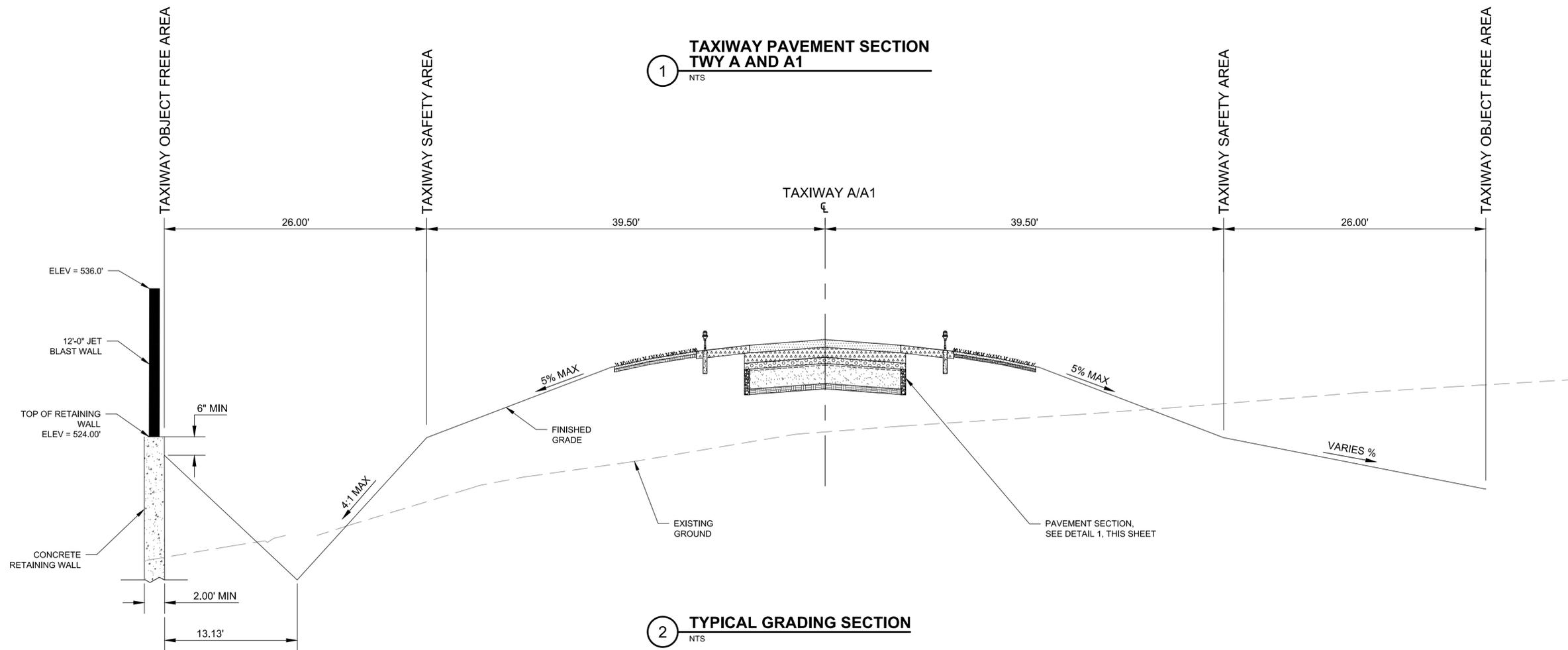


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**NOTE:**  
 1. CONTRACTOR MAY INSTALL P-209 AS A SUBSTITUTE FOR P-154. IF THE CONTRACTOR ELECTS TO PERFORM THIS SUBSTITUTION, THE P-209 MATERIAL WILL BE PAID AT THE LOWER UNIT PRICE OF EITHER P-154 OR P-209.

**1 TAXIWAY PAVEMENT SECTION TWY A AND A1**  
 NTS



**2 TYPICAL GRADING SECTION**  
 NTS

**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID



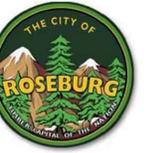
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SHEET CONTENTS  
 TYPICAL SECTIONS

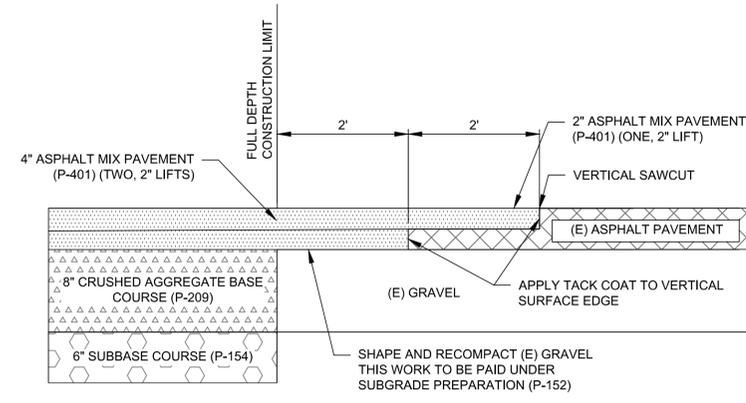
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**C-301**

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**NOTES:**

- SMOOTHNESS REQUIREMENTS FROM NEW TO EXISTING PAVEMENTS SHALL MEET THE REQUIREMENTS OF P-401-5.3.g.

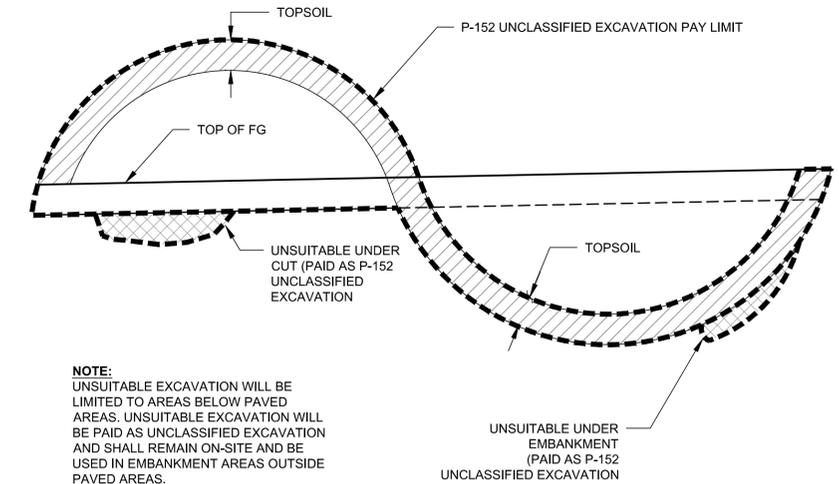
**1 PAVEMENT TRANSITION DETAIL TWY A AND A1**  
NTS

EXCAVATION TABLE (ESTIMATED QUANTITIES)		
UNCLASSIFIED EXCAVATION	TOPSOIL STRIPPING	1770 CY
	AGGREGATE BASE REMOVAL	810 CY
	SILTY CLAY	900 CY
EMBANKMENT	TOPSOILING	1600 CY
	AGGREGATE BASE (NO DIRECT PAYMENT)	810 CY
	SILTY CLAY (NO DIRECT PAYMENT)	765 CY
	FROM OPTIONAL BORROW SITE OR OFFSITE LOCATION (PAID AS EMBANKMENT IN PLACE, P-152)	2000 CY

**NOTES:**

- QUANTITIES IN TABLE ARE FOR ESTIMATED PURPOSES ONLY.
- UNCLASSIFIED EXCAVATION SHALL REMAIN ON-SITE AND BE USED AS EMBANKMENT MATERIAL IN THE AREAS ALLOWED PER THE PLANS AND SPECIFICATIONS.
- A SHRINKAGE FACTOR OF 15% HAS BEEN ASSUMED FOR EXCAVATED CLAY MATERIAL. A SHRINKAGE FACTOR OF 10% HAS BEEN ASSUMED FOR EXCAVATED TOPSOIL.

**2 EARTHWORK VOLUME**  
NTS



**NOTE:**

UNSUITABLE EXCAVATION WILL BE LIMITED TO AREAS BELOW PAVED AREAS. UNSUITABLE EXCAVATION WILL BE PAID AS UNCLASSIFIED EXCAVATION AND SHALL REMAIN ON-SITE AND BE USED IN EMBANKMENT AREAS OUTSIDE PAVED AREAS.

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ROSEBURG REGIONAL AIRPORT  
TAXIWAY A EXTENSION  
3896 NW STEWART PARKWAY  
ROSEBURG, OR 97470

ISSUED FOR BID



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SHEET CONTENTS  
TYPICAL SECTIONS

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**C-302**



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

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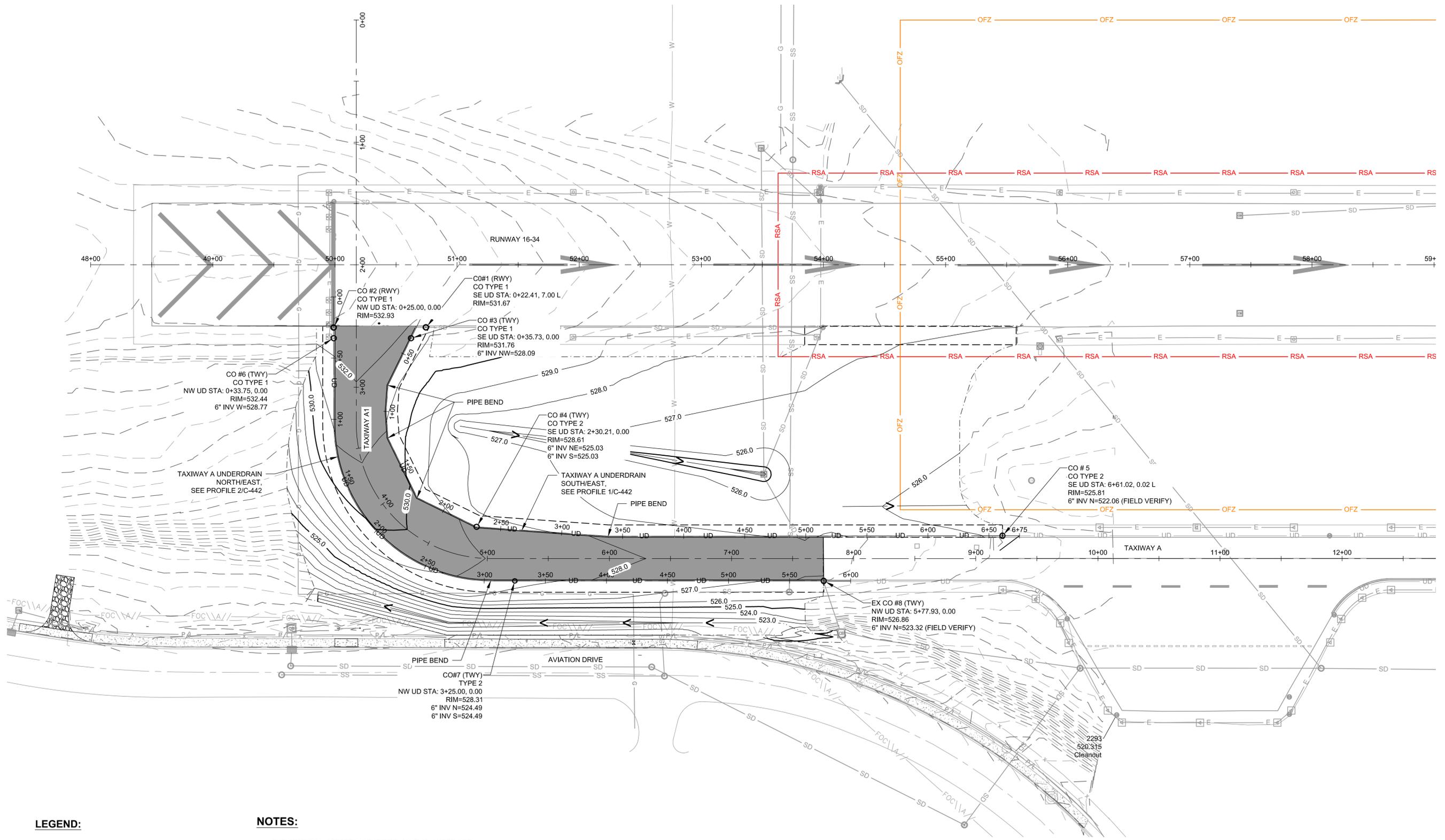


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SHEET CONTENTS  
UNDERDRAIN PLAN

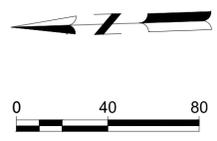
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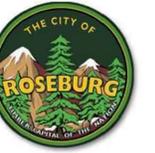


**LEGEND:**  
○ UNDERDRAIN CLEANOUT  
— UD — UNDERDRAIN

**NOTES:**  
1. THE STATION / OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE UD SE AND NW ALIGNMENTS.



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**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

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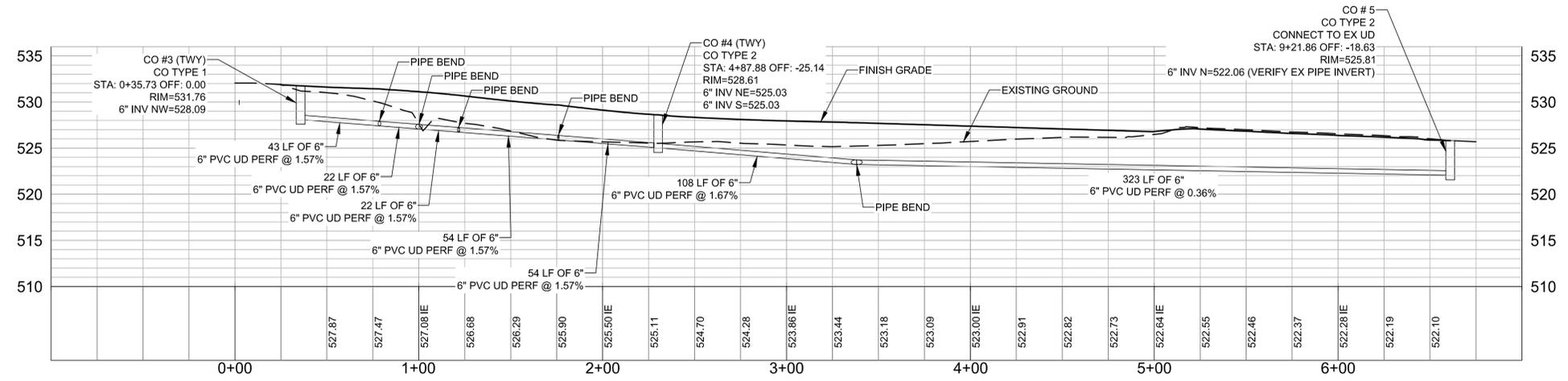


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 DO NOT SCALE DRAWINGS

SHEET CONTENTS  
UNDERDRAIN  
PROFILES

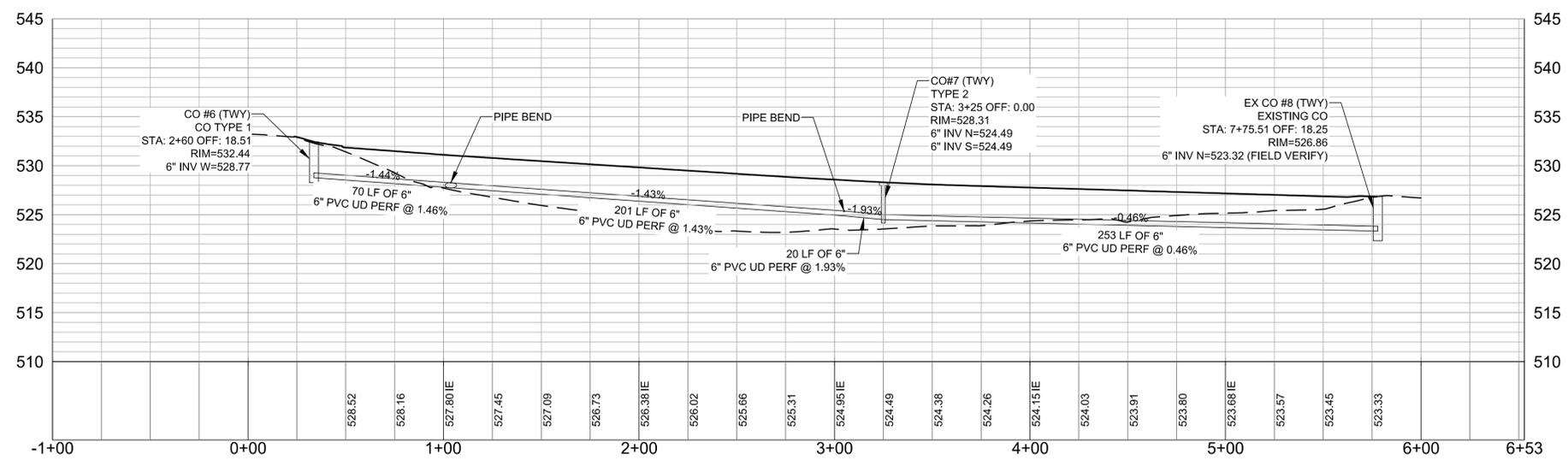
SHEET NO. 22 of 40

## C-442



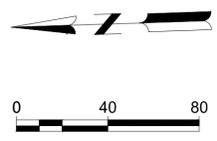
**NOTES:**  
1. THE STATION / OFFSETS IDENTIFIED REFERENCED OFF THE UD SE ALIGNMENT.

**1 TWY A SOUTH/EAST UNDERDRAIN PLAN**  
SCALE: 1" = 40'

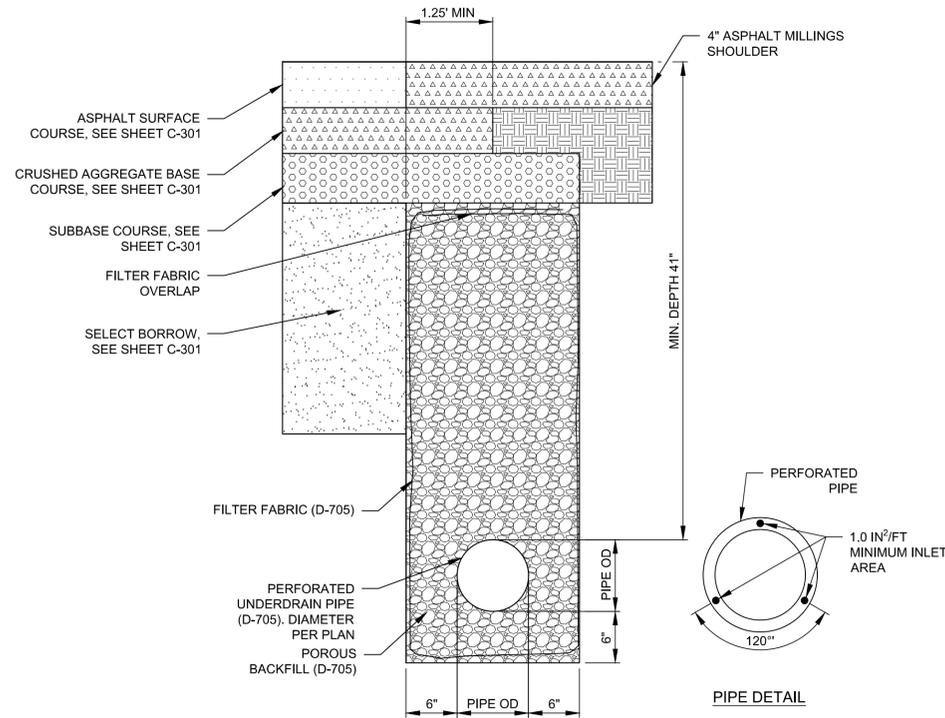


**NOTES:**  
1. THE STATION / OFFSETS IDENTIFIED REFERENCED OFF THE UD NW ALIGNMENT.

**2 TWY F NORTH/WEST UNDERDRAIN PROFILE**  
SCALE: 1" = 40'

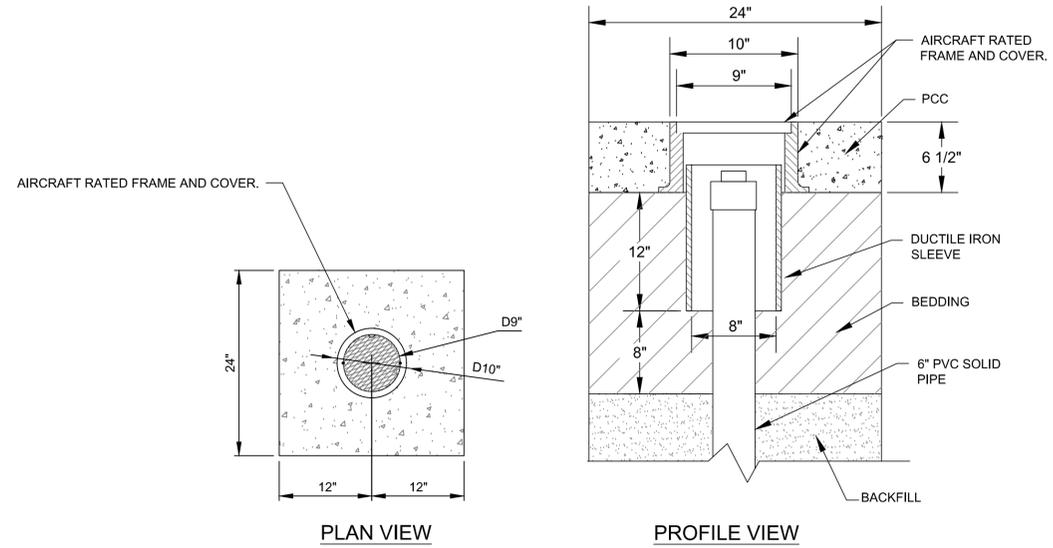


X:\1821200\221217.01\TECH\CAD\C-442 UNDERDRAIN PROFILES.DWG 4/17/2024 6:18:12 PM



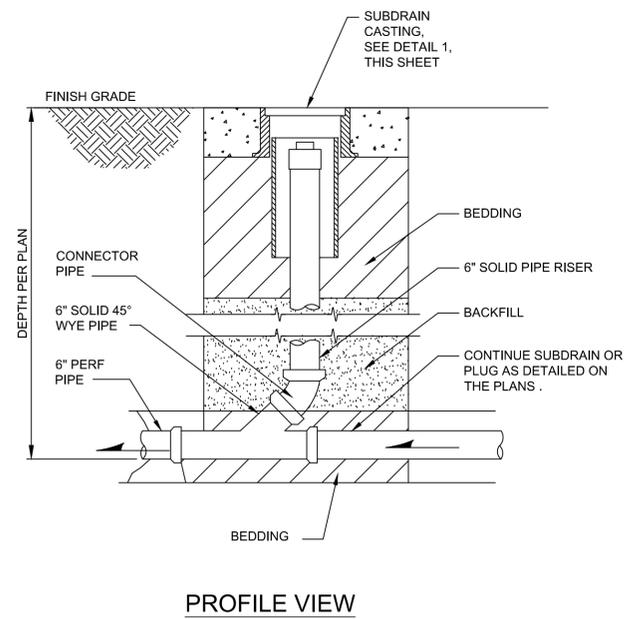
**NOTES:**

- HOLES IN PERFORATED PIPE SHALL BE ORIENTED AS SHOWN.
- CONTRACTOR IS RESPONSIBLE FOR PROTECTING THE ADJACENT PAVEMENT SECTION. ANY DAMAGE RESULTING FROM TRENCHING ACTIVITIES WILL BE REPAIRED AT THE EXPENSE OF THE CONTRACTOR.

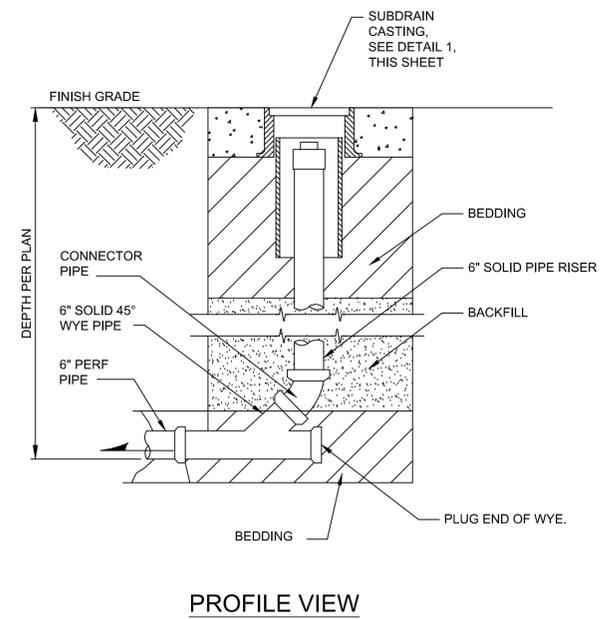


**1 UNDERDRAIN CASTING**  
NOT TO SCALE

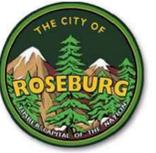
**3 PERFORATED UNDERDRAIN TRENCH**  
NOT TO SCALE



**4 CLEANOUT TYPE 2**  
NOT TO SCALE



**2 CLEANOUT TYPE 1**  
NOT TO SCALE



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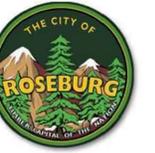
ISSUED FOR BID



AIP NO.: 3-41-0054-030-2024  
MSH NO.: 1821200-221217.01  
DATE: APRIL 18, 2024  
DESIGNED BY: MLF  
DRAWN BY: MJK  
CHECKED BY: RJB  
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SHEET CONTENTS  
UNDERDRAIN DETAILS

SHEET NO. 23 of 40



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

ISSUED FOR BID

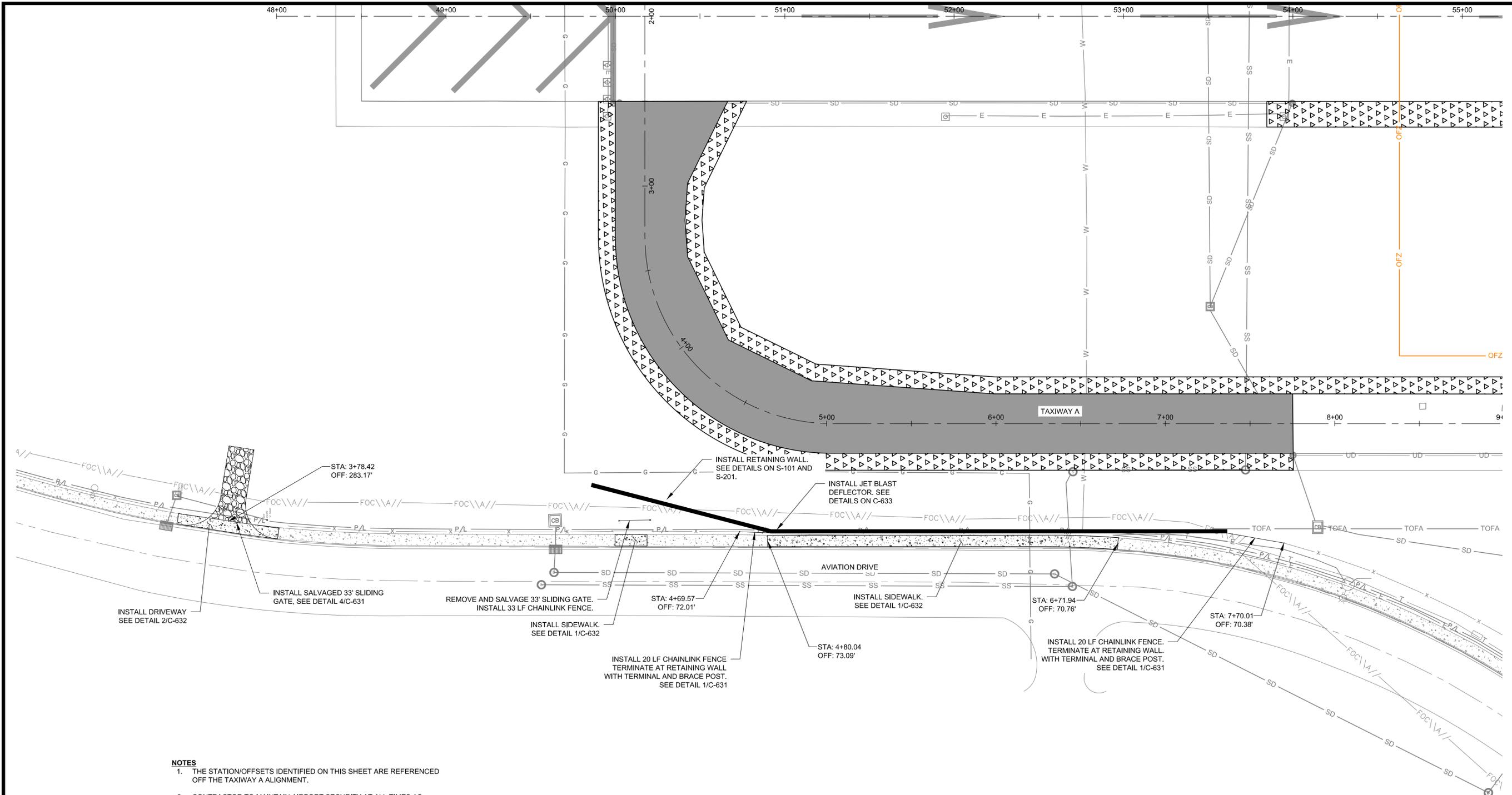


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DATE: APRIL 18, 2024  
DESIGNED BY: MLF  
DRAWN BY: MJK  
CHECKED BY: RJB  
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SHEET CONTENTS  
FENCING GATE & SIDEWALK PLAN

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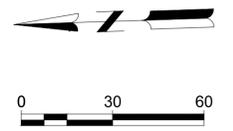
### C-601



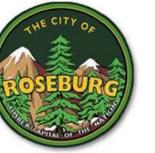
- NOTES**
1. THE STATION/OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE TAXIWAY A ALIGNMENT.
  2. CONTRACTOR TO MAINTAIN AIRPORT SECURITY AT ALL TIMES AS DEFINED IN THE CSPP. CONTRACTOR TO PROVIDE A WORK PLAN DEPICTING SEQUENCE OF EVENTS AND MAINTENANCE OF AIRPORT SECURITY PRIOR TO FENCE REMOVAL.
  3. AT THE CONTRACTOR'S OPTION, 6-FOOT CHAIN-LINK FENCE MATERIALS REMOVED UNDER THIS PROJECT MAY BE SALVAGED AND REUSED TO CONSTRUCT 6-FOOT CHAIN-LINK FENCE CONTAINED UNDER OTHER ITEMS OF WORK. THESE MATERIALS ARE LIMITED TO CHAIN-LINK FENCE FABRIC, BARBED WIRE EXTENSION ARMS, TOP RAIL, AND GATE PANELS. SALVAGEABLE IS CONSIDERED FABRIC WITHOUT HOLES, TEARS, SNAGS, ETC.; EXTENSION ARMS WITHOUT KINKS OR BROKEN BRACKETS; TOP RAIL WITHOUT KINKS; AND GATE PANELS THAT, WHEN HUNG, PROVIDE THE FUNCTIONALITY OF A NEW GATE. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING ALL SALVAGED COMPONENTS ARE COMPATIBLE WITH THE NEW MATERIALS BEING PROVIDED.

**LEGEND:**

CONCRETE SIDEWALK / DRIVEWAY WITH CURB



X:\1821200\221217.01\TECH\CAD\C-601 FENCING GATE & SIDEWALK PLAN.DWG 4/17/2024 6:18:31 PM



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**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID

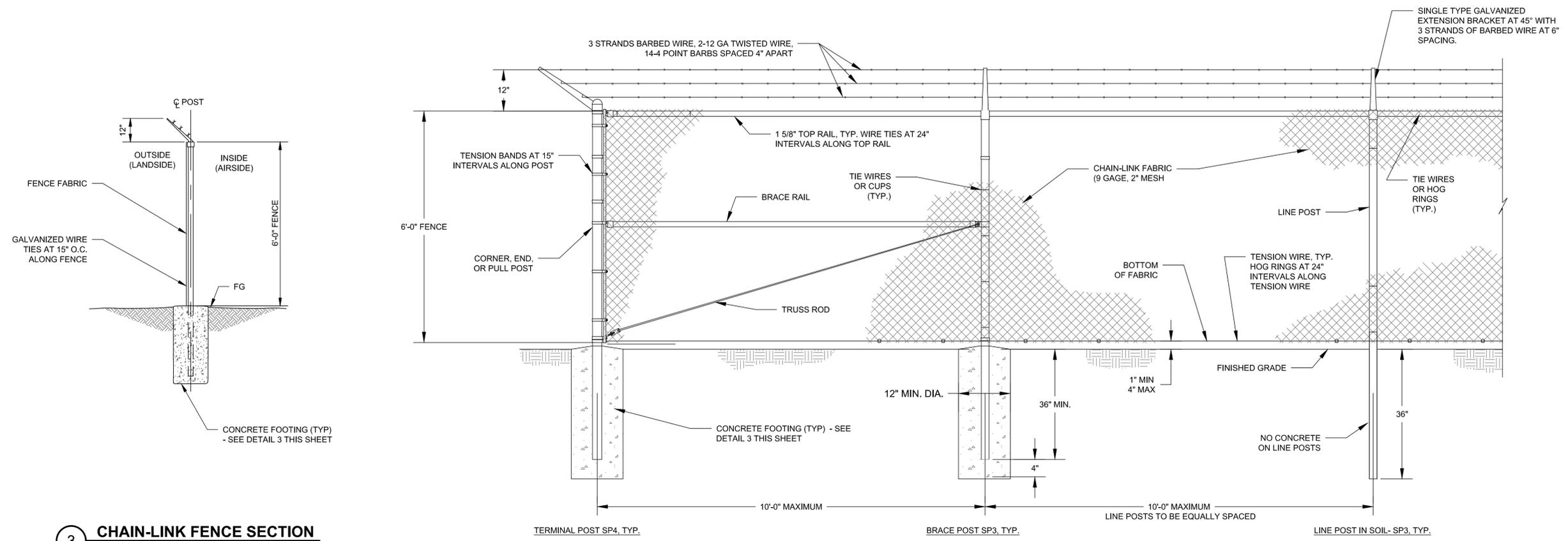


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 DATE: APRIL 18, 2024  
 DESIGNED BY: MLF  
 DRAWN BY: MJK  
 CHECKED BY: RJB  
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SHEET CONTENTS  
**FENCING & GATE DETAILS**

SHEET NO. 25 of 40

**C-631**

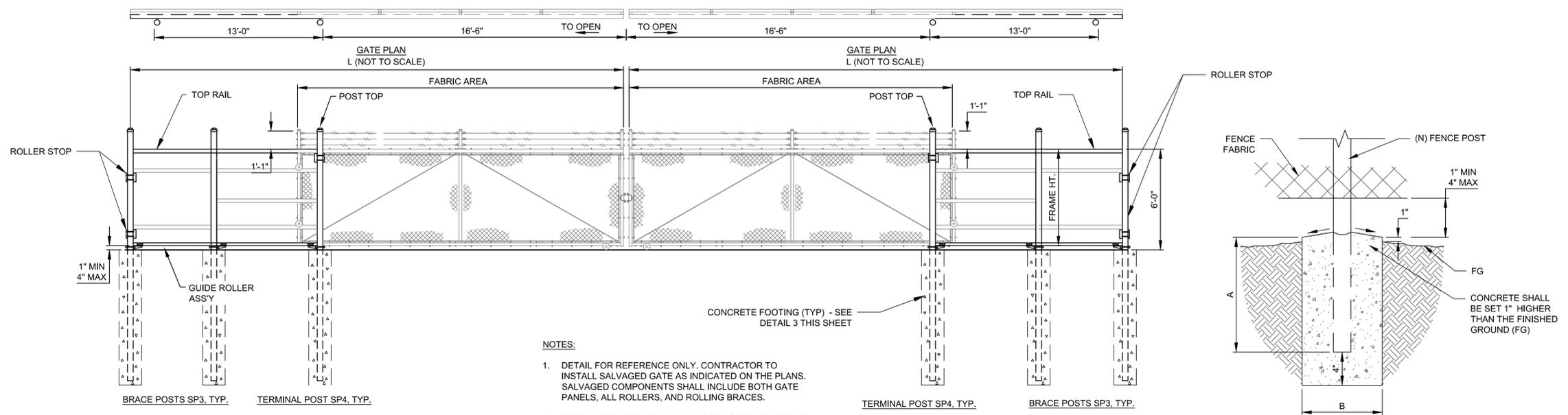


**3 CHAIN-LINK FENCE SECTION**  
NOT TO SCALE

**1 CHAIN-LINK SECURITY FENCE DETAIL**  
NOT TO SCALE

**FENCE NOTES:**

- AT THE CONTRACTOR'S OPTION, 6-FOOT CHAIN-LINK FENCE MATERIALS REMOVED UNDER THIS PROJECT MAY BE SALVAGED AND REUSED TO CONSTRUCT 6-FOOT CHAIN-LINK FENCE CONTAINED UNDER OTHER ITEMS OF WORK. THESE MATERIALS ARE LIMITED TO CHAIN-LINK FENCE FABRIC, BARBED WIRE EXTENSION ARMS, TOP RAIL, AND GATE PANELS. SALVAGEABLE IS CONSIDERED FABRIC WITHOUT HOLES, TEARS, SNAGS, ETC.; EXTENSION ARMS WITHOUT KINKS OR BROKEN BRACKETS; TOP RAIL WITHOUT KINKS; AND GATE PANELS THAT, WHEN HUNG, PROVIDE THE FUNCTIONALITY OF A NEW GATE. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING ALL SALVAGED COMPONENTS ARE COMPATIBLE WITH THE NEW MATERIALS BEING PROVIDED.



**4 SALVAGED MANUAL DOUBLE SLIDING GATE**  
NOT TO SCALE

**2 TYPICAL FENCE POST FOOTING**  
NOT TO SCALE

**FENCE FOOTING AND MATERIALS NOTES:**

- CONCRETE FOOTING DEPTH (A)  
6'-0" CHAINLINK FENCE  
• TERMINAL POSTS & GATE POSTS - 3'-6"  
• BRACE POSTS - 3'-0"
- CONCRETE FOOTING WIDTH (B)  
• TERMINAL POSTS & GATE POSTS - 18"  
• BRACE POSTS - 12"
- CONCRETE SHALL BE OF A COMMERCIAL GRADE WITH MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3,000 PSI.
- POSTS, RAILS AND BRACING SIZES SHALL CONFORM TO THE SIZES LISTED IN THE TABLE BELOW OF FED SPEC RR-F-191/3. ALL OTHER FENCING MATERIALS SHALL CONFORM TO THE RELEVANT FED SPEC FOR THAT MATERIAL.
- CONCRETE TO HAVE A SMOOTH FINISH.

**POST SIZES PER FED SPEC RR-F-191/3.**

FED SPEC SIZE	OUTSIDE DIAMETER	MIN WALL THICKNESS
SP2	1.900"	0.120"
SP3	2.375"	0.130"
SP4	2.875"	0.160"
SP5	4.000"	0.226"
SP6	6.625"	0.280"

X:\1821200\221217.01\TECH\CAD\C-631 FENCING & GATE DETAILS.DWG 4/17/2024 6:18:39 PM



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**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID

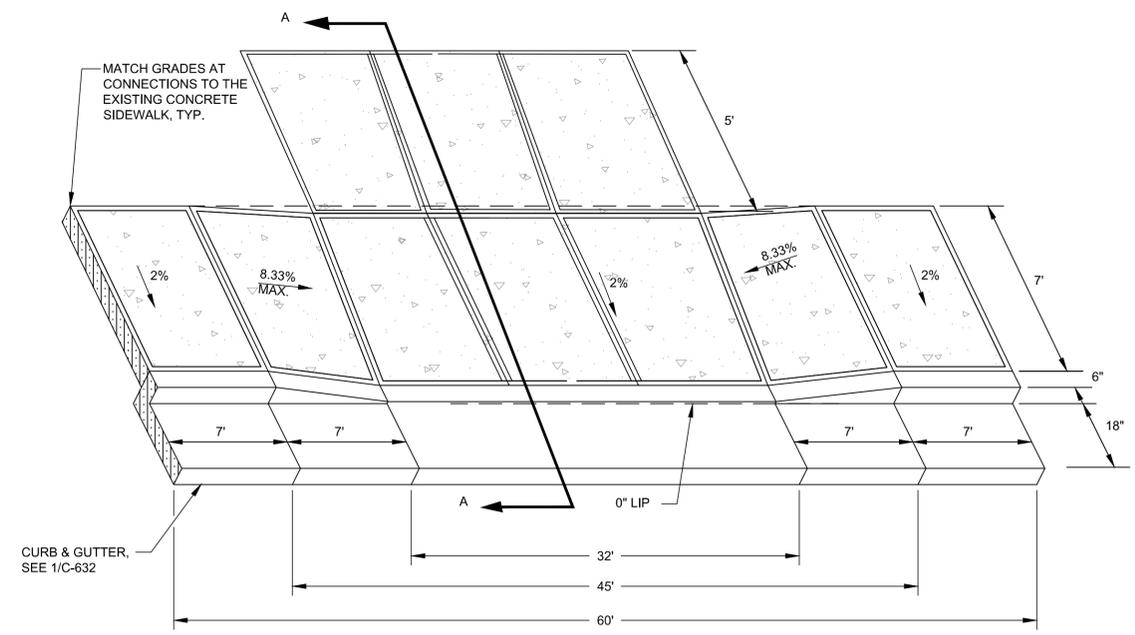


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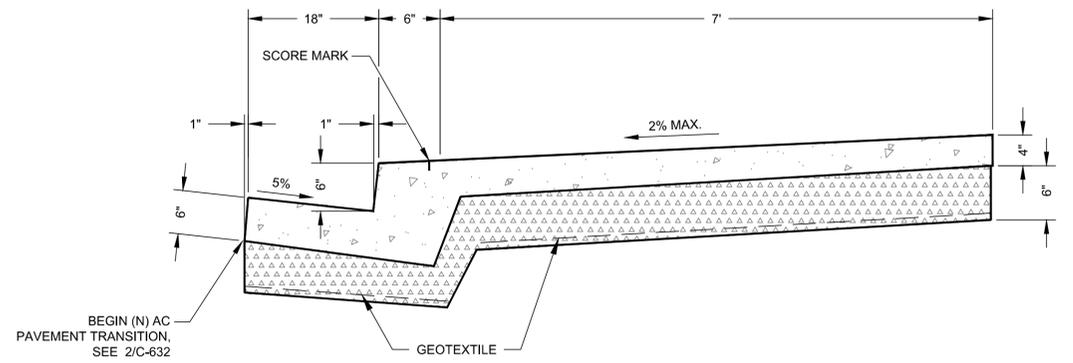
SHEET CONTENTS  
 SIDEWALK &  
 CONCRETE  
 ENTRANCE DETAILS

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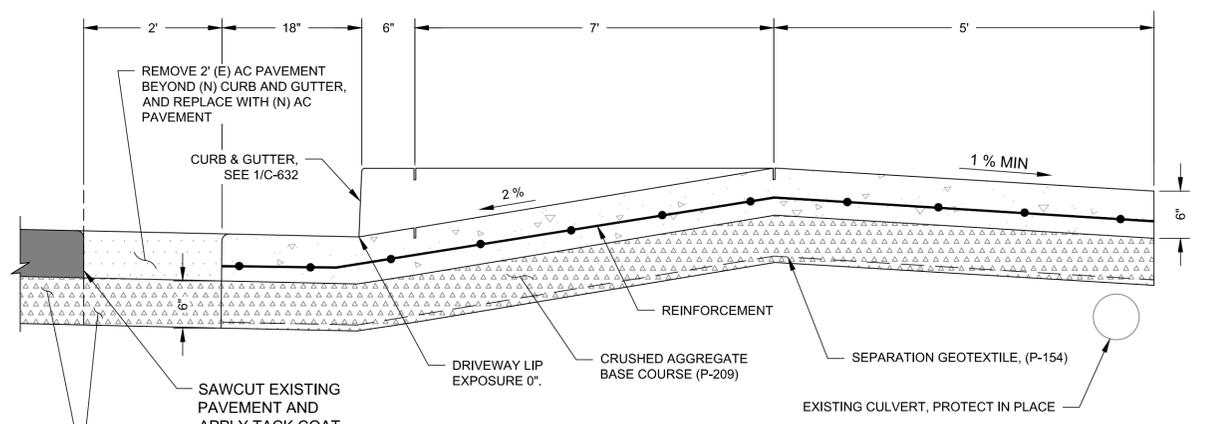
## C-632



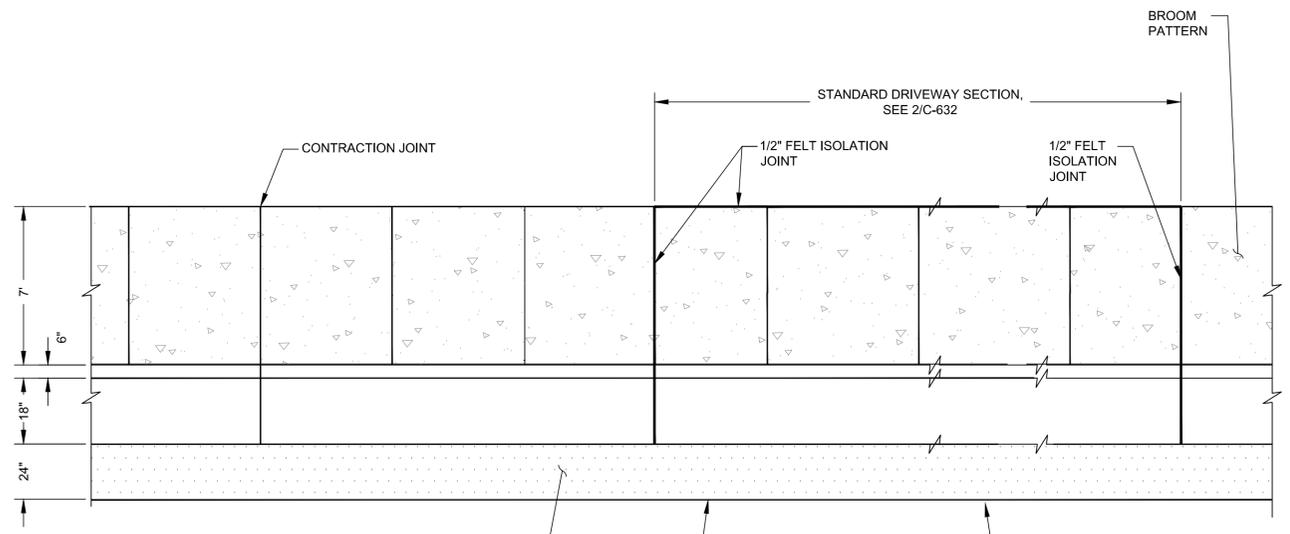
DRIVEWAY APPROACH PLAN



SIDEWALK WITH CURB AND GUTTER CROSS SECTION



SECTION A-A  
DRIVEWAY APPROACH



SIDEWALK PLAN

**NOTES:**

- SIDEWALK SECTIONS THROUGH THE DRIVEWAY APPROACH SHALL HAVE A MINIMUM NOMINAL THICKNESS OF 6".
- CONCRETE SHALL MEET THE REQUIREMENTS OF ITEM P-610
- CURB TRANSITIONS FOR DRIVEWAY APPROACHES SHALL BE 7 FEET.
- REINFORCEMENT SHALL BE #4 REBAR. REBAR SHALL BE PLACED 12" O.C. EACH WAY, FOR THE FULL 60 LF DRIVEWAY SECTION.

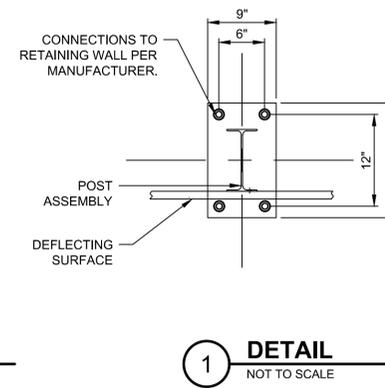
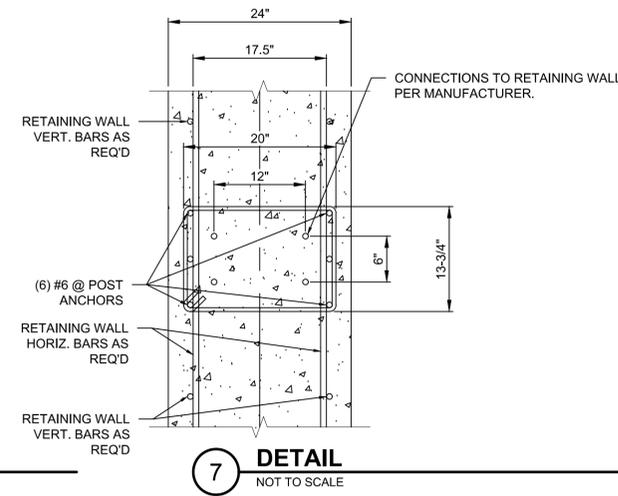
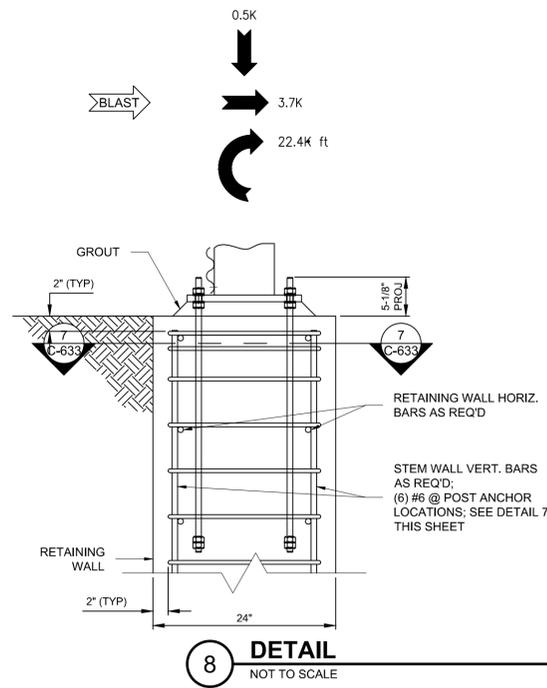
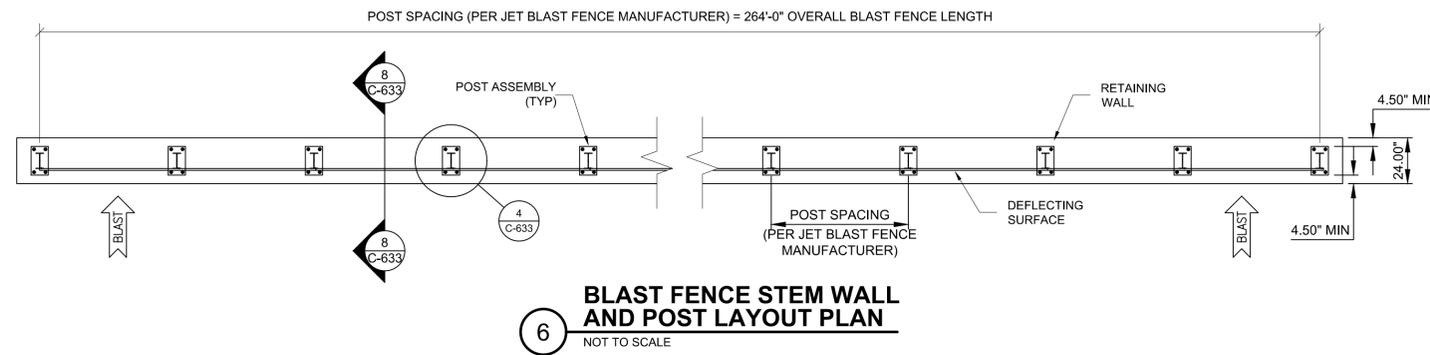
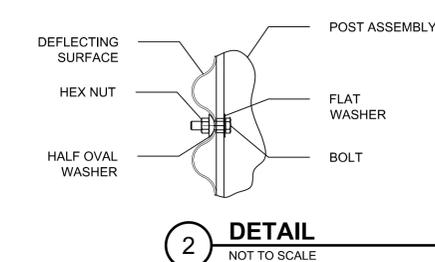
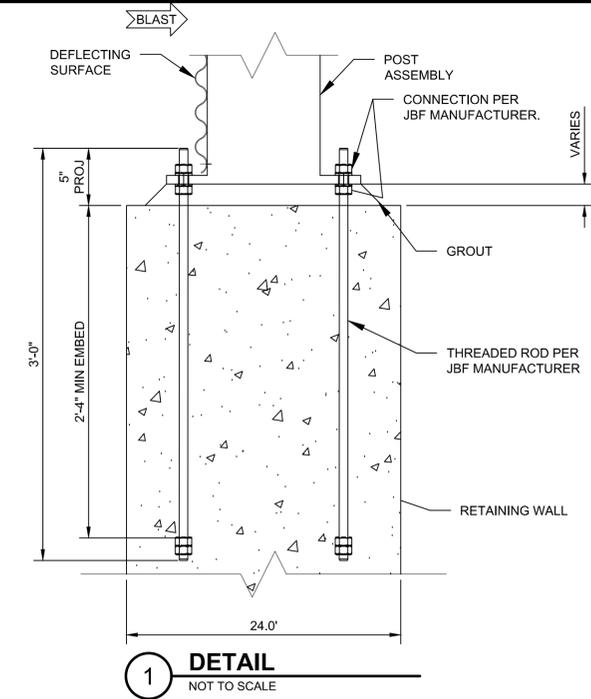
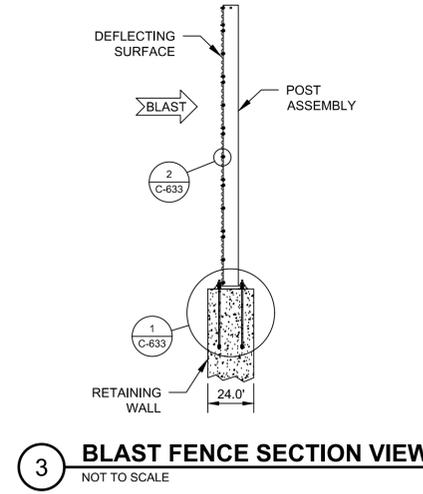
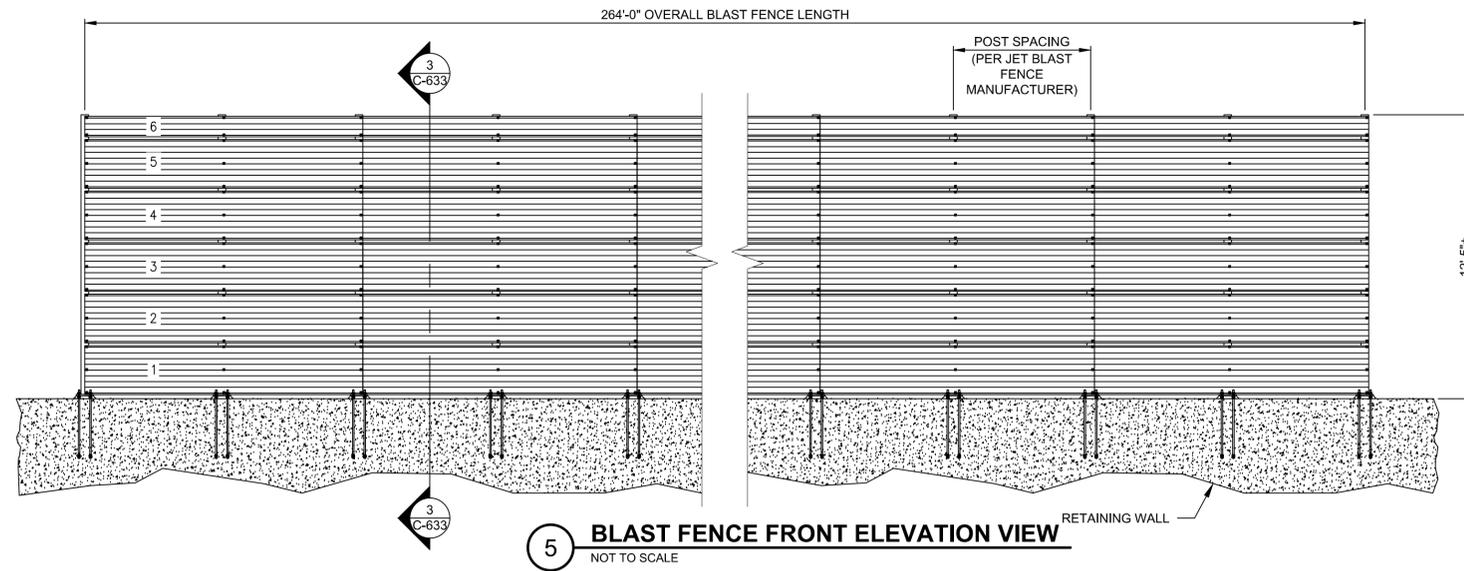
**NOTES:**

- CONCRETE USED IN SIDEWALKS SHALL MEET THE REQUIREMENTS OF ITEM P-610.
- ALL RADII SHALL BE 3/4" UNLESS OTHERWISE SHOWN.
- ISOLATION JOINTS (FELT EXPANSION JOINTS) SHALL BE PLACED AT 45' INTERVALS.
- CONTRACTION JOINTS SHALL BE PLACED AT 15' INTERVALS AND SHALL EXTEND THROUGH THE CURB OR CURB AND GUTTER EVERY OTHER ONE.

**2 CONCRETE DRIVEWAY ENTRANCE**  
NOT TO SCALE

**1 CONCRETE SIDEWALK DETAIL**  
NOT TO SCALE

X:\1821200\221217.01\TECH\CAD\C-632 SIDEWALK & CONCRETE ENTRANCE DETAILS.DWG 4/17/2024 6:18:52 PM



**BLAST FENCE NOTES:**

- BLAST FENCE SHALL WITHSTAND TAXI/BREAKAWAY EXHAUST VELOCITIES OF GENERAL AVIATION AND BUSINESS JET AIRCRAFT. DESIGN LOADS ARE:  
140 MPH JET BLAST PER FAA GUIDELINES  
50 PSF (NOM.)  
97 MPH (ULT.) WIND PER THE 2022 OSSC / ASCE 7-1647.1  
PSF (ULT.) = 28.3 PSF (NOM.)
- CONTRACTOR TO SUBMIT SHOP DRAWINGS FROM JET BLAST FENCE (JBF) MANUFACTURER TO BE REVIEWED BY THE ENGINEER. MANUFACTURER MUST HAVE DEMONSTRATED PRIOR EXPERIENCE DESIGNING AND PROVIDING JET BLAST FENCE DESIGNED SPECIFICALLY FOR AIRPORTS.
- DRAWINGS AND DETAILS SHOWN ON THIS SHEET ARE FOR REFERENCE ONLY. OTHER DESIGNS MAY BE USED THAT MEET THE PERFORMANCE SPECIFICATIONS IDENTIFIED.
- ALL FIELD CONNECTIONS SHALL BE BOLTED (NO FIELD WELDING PERMITTED). FASTENERS SHALL HAVE AN APPROPRIATE COATING FOR CORROSION RESISTANCE (WHERE APPLICABLE). ADEQUATE LOCKING PROPERTIES SHALL BE PROVIDED TO PREVENT FASTENERS FROM WORKING LOOSE DURING NORMAL OPERATION (SUBJECT TO MANUFACTURER MAINTENANCE GUIDELINES).
- ALL ANCHORAGE SHALL BE SUPPLIED BY THE BLAST FENCE MANUFACTURER AND SHALL BE CAST-IN-PLACE BY THE CONTRACTOR DURING RETAINING WALL CONSTRUCTION.
- BLAST FENCE MANUFACTURER ONSITE SUPERVISION IS REQUIRED DURING INSTALLATION.



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DATE: APRIL 18, 2024  
DESIGNED BY: MLF  
DRAWN BY: MJK  
CHECKED BY: RJB  
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SHEET CONTENTS  
JET BLAST  
DEFLECTOR DETAILS

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**C-633**



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**ROSEBURG REGIONAL AIRPORT  
TAXIWAY A EXTENSION**  
3896 NW STEWART PARKWAY  
ROSEBURG, OR 97470

ISSUED FOR BID

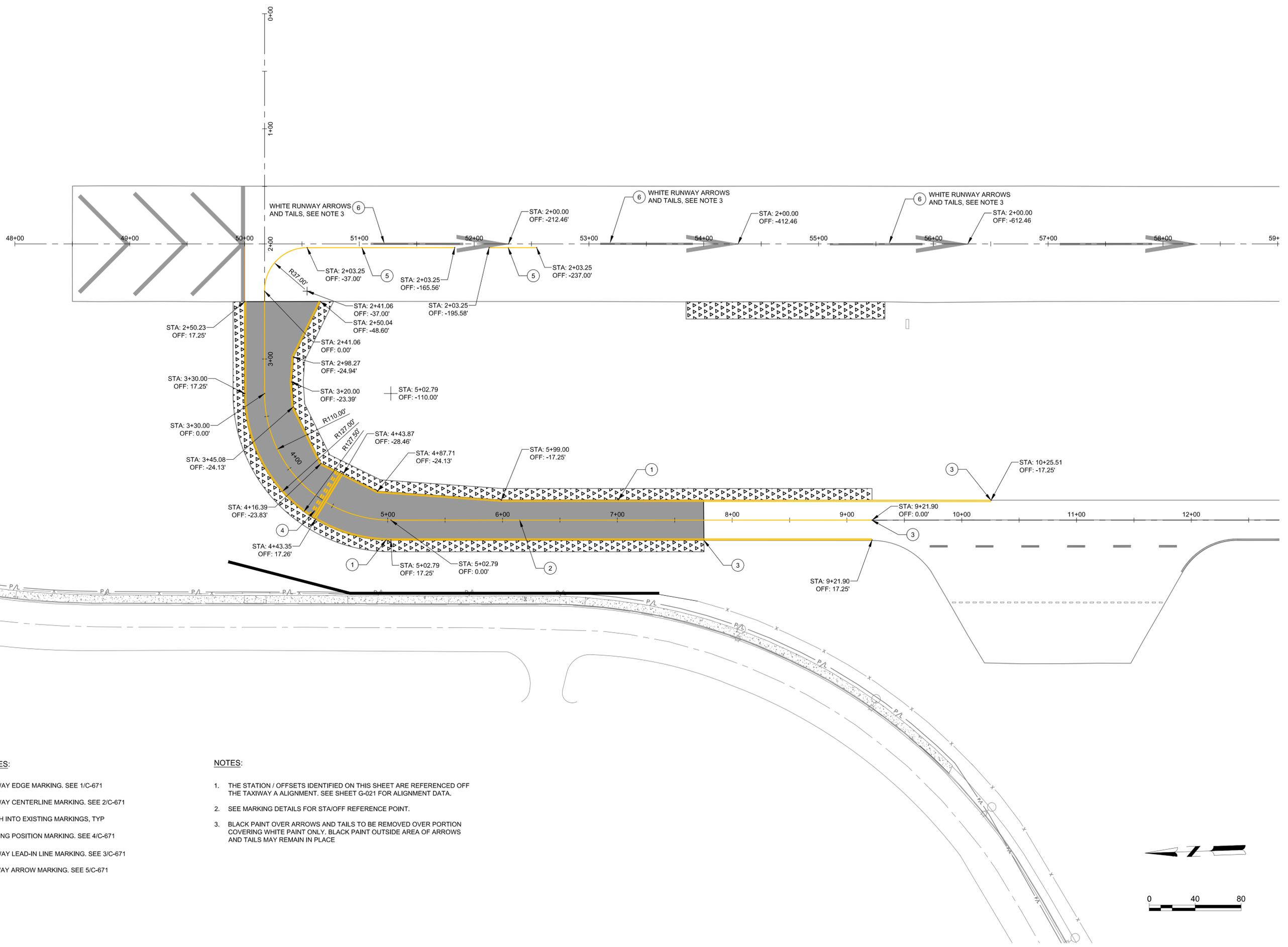


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DRAWN BY: MJK  
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SHEET CONTENTS  
MARKING PLAN

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**C-651**



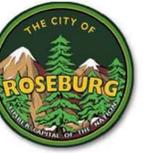
**KEY NOTES:**

- ① TAXIWAY EDGE MARKING. SEE 1/C-671
- ② TAXIWAY CENTERLINE MARKING. SEE 2/C-671
- ③ MATCH INTO EXISTING MARKINGS, TYP
- ④ HOLDING POSITION MARKING. SEE 4/C-671
- ⑤ TAXIWAY LEAD-IN LINE MARKING. SEE 3/C-671
- ⑥ RUNWAY ARROW MARKING. SEE 5/C-671

**NOTES:**

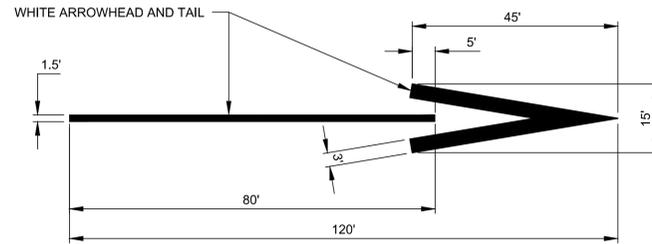
1. THE STATION / OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE TAXIWAY A ALIGNMENT. SEE SHEET G-021 FOR ALIGNMENT DATA.
2. SEE MARKING DETAILS FOR STA/OFF REFERENCE POINT.
3. BLACK PAINT OVER ARROWS AND TAILS TO BE REMOVED OVER PORTION COVERING WHITE PAINT ONLY. BLACK PAINT OUTSIDE AREA OF ARROWS AND TAILS MAY REMAIN IN PLACE

X:\1821200\221217.01\TECH\CAD\C-651 MARKING PLAN.DWG 4/17/2024 6:19:13 PM

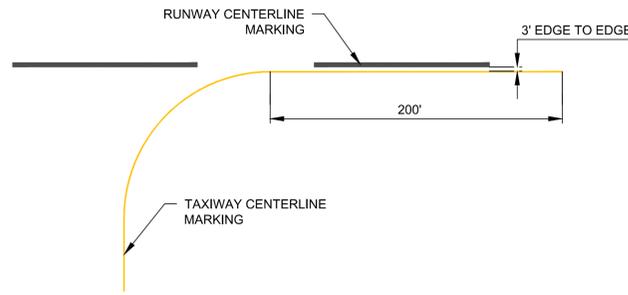


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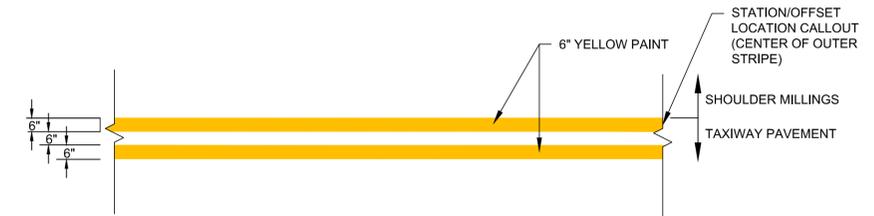
PAINT TYPE	PAINT MAXIMUM APPLICATION RATE (FT <sup>2</sup> /GAL)	GLASS BEAD MINIMUM APPLICATION RATE (LBS/GAL)
YELLOW MARKING	115	7
INITIAL YELLOW MARKING	230	X
BLACK MARKING	115	X
WHITE MARKING	115	7



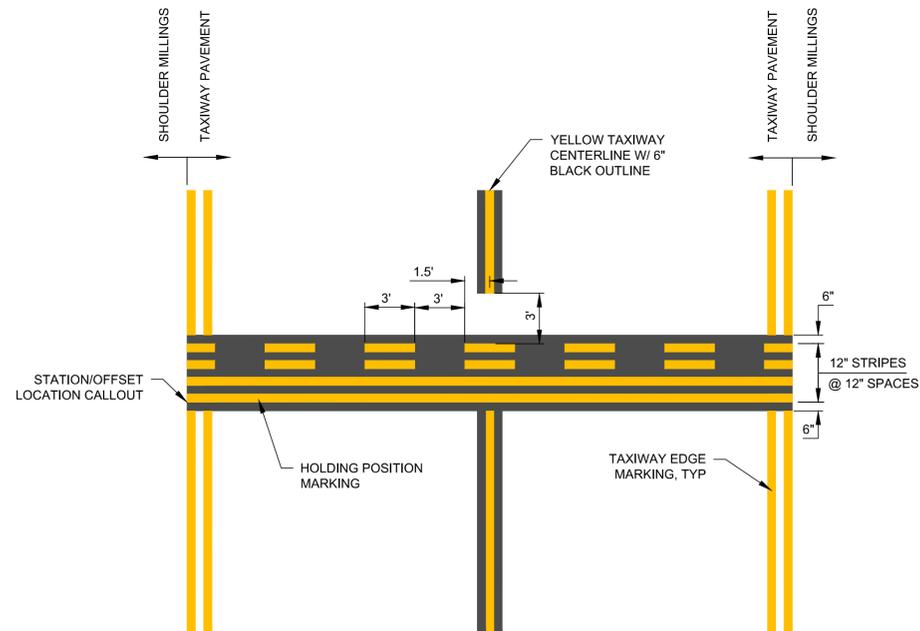
**5 ARROW MARKING**  
NO SCALE



**3 TAXIWAY LEAD-IN LINE**  
NOT TO SCALE



**1 TAXIWAY EDGE MARKING**  
NOT TO SCALE



**4 HOLDING POSITION MARKING**  
NOT TO SCALE



**2 TAXIWAY CENTERLINE MARKING**  
NOT TO SCALE

**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID



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SHEET CONTENTS  
MARKING DETAILS

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**C-671**



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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

ISSUED FOR BID

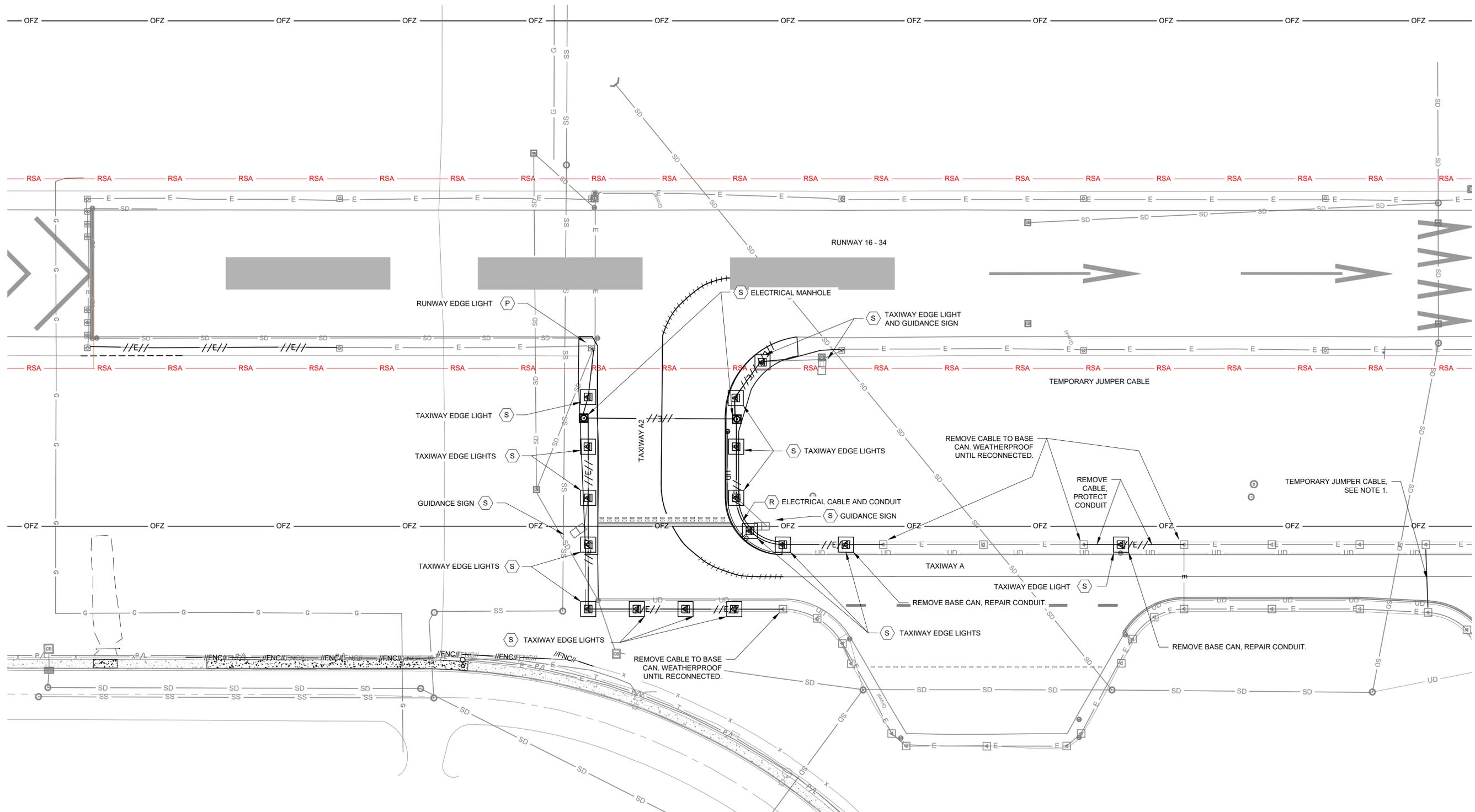


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DRAWN BY: MJK  
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SHEET CONTENTS  
ELECTRICAL  
REMOVAL PLAN

SHEET NO. 30 of 40

# E-101



### NOTES:

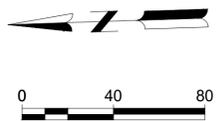
- TEMPORARY JUMPER CABLE TO BE INSTALLED IN A 1" PVC CONDUIT ABOVE GROUND. 2 2X8 PLANKS, WITH SANDBAGS SHALL BE USED TO FASTEN CONDUIT TO PAVEMENT TO PROTECT JUMPER FROM DAMAGE. OTHER METHODS MAY BE USED WITH APPROVAL BY THE RPR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE ACTUAL LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION. NEITHER THE OWNER OR THE ENGINEER ASSUME ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR COMPLETENESS OF THE INFORMATION PROVIDED.
- THE CONTRACTOR SHALL NOTIFY THE PROPER OWNER OF THEIR OPERATIONAL PLANS AFFECTING EACH UTILITY. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING ALL UTILITIES WITH THE RESPECTIVE RESPONSIBLE PARTIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RESPECTIVE COMPANY AND THE ENGINEER. ANY UTILITIES DISTURBED BY THE CONTRACTOR'S CARELESSNESS SHALL BE RESTORED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE OWNER AND ENGINEER.
- EXISTING LIGHTING EQUIPMENT AND SYSTEMS NOT REMOVED SHALL BE PROTECTED AND MAINTAINED BY THE CONTRACTOR.
- PROTECT EXISTING AIRPORT FACILITIES, CIRCUITS, COUNTERPOISE, EQUIPMENT AND UTILITIES DURING NEW INSTALLATION. ANY DAMAGE DONE TO EXISTING UTILITIES SHALL BE REPORTED TO THE ENGINEER AND PROMPTLY REPAIRED OR REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE OWNER AND ENGINEER, AT NO EXPENSE TO THE AIRPORT.
- THE CONTRACTOR WILL BE REQUIRED TO PROMPTLY REPAIR CONSTRUCTION DAMAGED PAVEMENT ADJACENT TO PROJECT LIMITS.
- REMOVED ITEMS SHALL BE STOCKPILED IN A LOCATION AGREED BY THE AIRPORT. THE AIRPORT AND FAA WILL HAVE 5 BUSINESS DAYS TO SALVAGE ANY EQUIPMENT. AFTER THE 5 BUSINESS DAYS, ALL REMAINING ITEMS BECOME PROPERTY OF THE CONTRACTOR TO DISPOSE OF OFF SITE. THE CONTRACTOR SHALL RESTORE THE AREA TO PREVIOUS CONDITIONS. THIS IS INCIDENTAL TO THE REMOVAL BID ITEM.
- THE CONTRACTOR WILL BE REQUIRED TO MEGGER TEST ALL EXISTING CIRCUITS BEFORE AND AFTER CONSTRUCTION TO ENSURE PROPER INSULATION. MEGGER TESTS MUST BE WITNESSED BY ENGINEER, AND WRITTEN TEST RESULTS MUST ALSO BE SUBMITTED TO THE ENGINEER. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL INFORMATION.

### SHEET KEYNOTES:

- (A) ABANDON
- (R) REMOVE
- (P) PROTECT IN PLACE
- (S) SALVAGE

### LEGEND:

- //E// — EXISTING CABLE AND CONDUIT
- ▬ AIRFIELD GUIDANCE SIGN



X:\1821200\221217\_01\TECH\CADE-102 EXISTING SIGN REMOVAL PLAN.DWG  
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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

ISSUED FOR BID

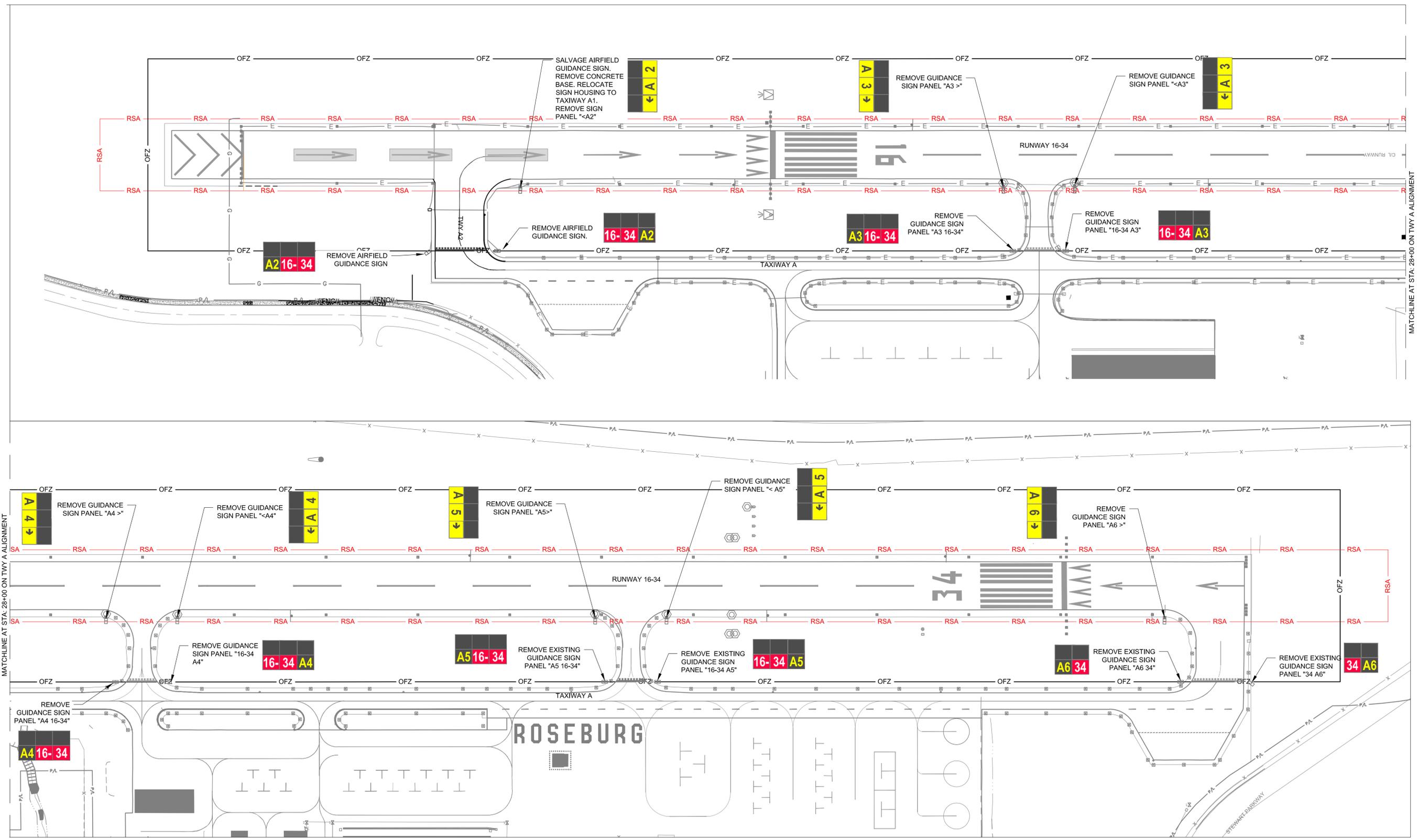


AIP NO: 3-41-0054-030-2024  
MSH NO: 1821200-221217.01  
DATE: APRIL 18, 2024  
DESIGNED BY: MLF  
DRAWN BY: MJK  
CHECKED BY: RJB  
DO NOT SCALE DRAWINGS

SHEET CONTENTS  
EXISTING SIGN  
REMOVAL PLAN

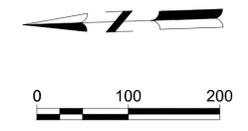
SHEET NO. 31 of 40

### E-102

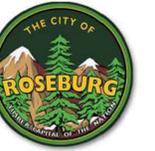


**NOTES:**

1. FOR INSTALLATION OF TEMPORARY JUMPER, SEE G-081.
2. SEE SHEET E-202 FOR RELOCATED SIGN POSITIONS.
3. ALL REMOVED PANELS, GUIDANCE SIGNS, AND ASSOCIATED PARTS TO BE KEPT ONSITE IN A DRY PLACE. CITY OF ROSEBURG AT THEIR DISCRETION MAY CHOOSE TO TAKE OWNERSHIP OF ITEMS ONCE THEY HAVE BEEN REMOVED.



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**ROSEBURG REGIONAL AIRPORT  
TAXIWAY A EXTENSION**  
3896 NW STEWART PARKWAY  
ROSEBURG, OR 97470

ISSUED FOR BID



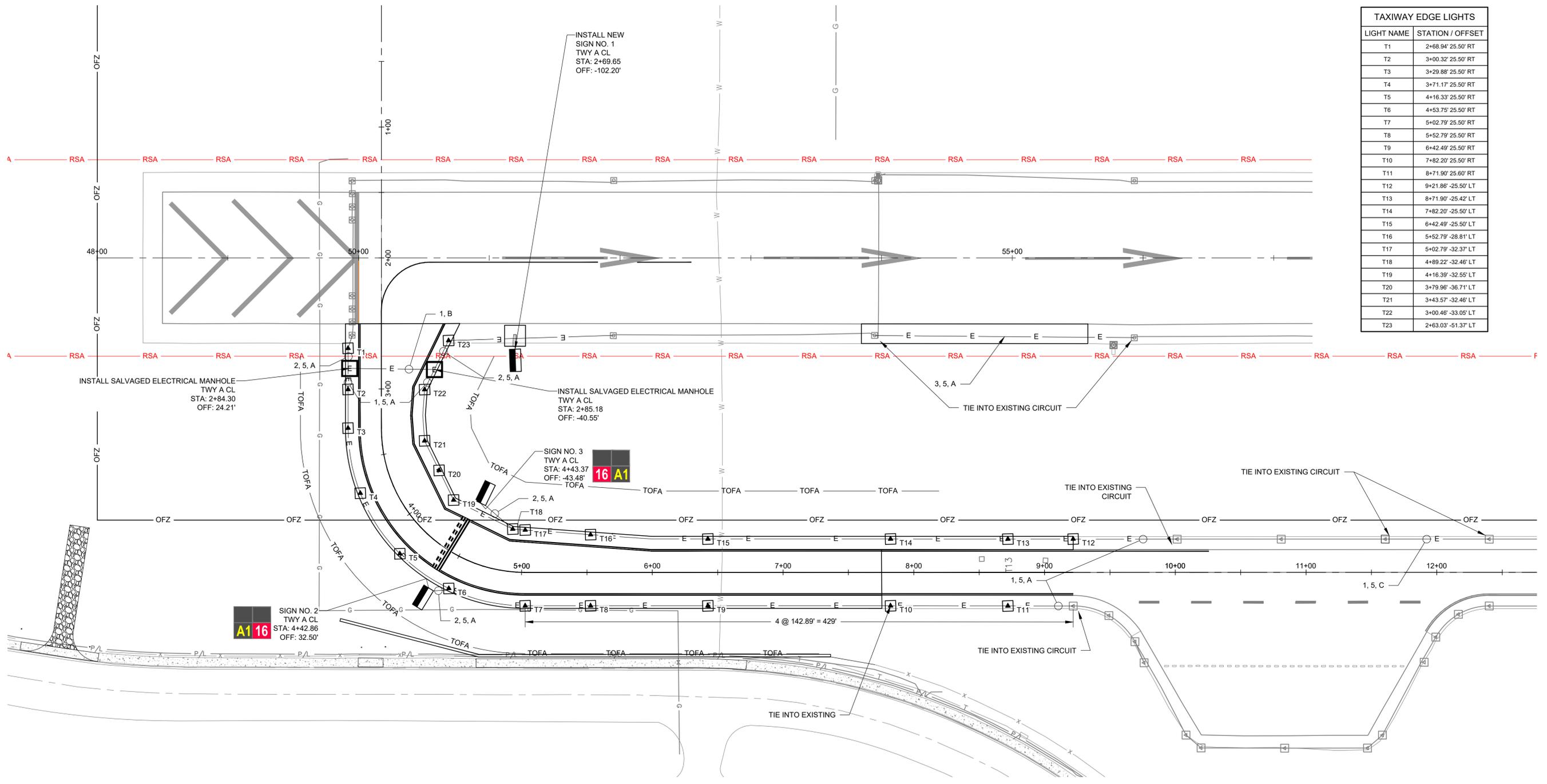
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SHEET CONTENTS  
ELECTRICAL LAYOUT PLAN

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**E-201**

TAXIWAY EDGE LIGHTS	
LIGHT NAME	STATION / OFFSET
T1	2+68.94' 25.50' RT
T2	3+00.32' 25.50' RT
T3	3+29.88' 25.50' RT
T4	3+71.17' 25.50' RT
T5	4+16.33' 25.50' RT
T6	4+53.75' 25.50' RT
T7	5+02.79' 25.50' RT
T8	5+52.79' 25.50' RT
T9	6+42.49' 25.50' RT
T10	7+82.20' 25.50' RT
T11	8+71.90' 25.60' RT
T12	9+21.86' -25.50' LT
T13	8+71.90' -25.42' LT
T14	7+82.20' -25.50' LT
T15	6+42.49' -25.50' LT
T16	5+52.79' -28.81' LT
T17	5+02.79' -32.37' LT
T18	4+89.22' -32.46' LT
T19	4+16.39' -32.55' LT
T20	3+79.96' -36.71' LT
T21	3+43.57' -32.46' LT
T22	3+00.46' -33.05' LT
T23	2+63.03' -51.37' LT



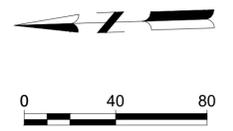
CIRCUIT CHART		
LABEL	NUMBER AND TYPE OF CABLE	CIRCUIT
1	1 - 1/C, #8 5KV, L-824, TYPE C	TWY A
2	2 - 1/C, #8 5KV, L-824, TYPE C	TWY A
3	1 - 1/C, #8 5KV, L-824, TYPE C	RWY 16-34
4	2 - 1/C, #8 5KV, L-824, TYPE C	RWY 16-34
5	#6 COUNTERPOISE	
NUMBER AND TYPE OF CONDUIT		
A	1W-2" PVC CONDUIT	
B	1W-2" PVC CONDUIT, CONCRETE ENCASED	
C	EXISTING CONDUIT	

**LEGEND:**

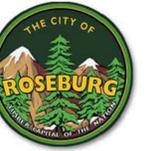
	MEDIUM INTENSITY TAXIWAY EDGE LIGHT
	MEDIUM INTENSITY RUNWAY EDGE LIGHT
	ELECTRICAL JUNCTION BOX
	NEW ELECTRICAL CABLE AND CONDUIT
	AIRFIELD GUIDANCE SIGN

- NOTES:**
1. THE STATION / OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE TAXIWAY A ALIGNMENT.
  2. THE STATION / OFFSETS IDENTIFIED FOR GUIDANCE SIGNS REFER TO THE CENTER OF THE EDGE NEAREST TO THE TAXIWAY PAVEMENT EDGE.
  3. SEE PHASING PLAN FOR TEMPORARY JUMPER CABLE LOCATION. DETAIL 4 ON E-602.
  4. CONTRACTOR SHALL PERMANENTLY LABEL EACH CIRCUIT INSIDE ELECTRICAL BASE CANS AND HANDHOLES.
  5. ALL OFFSET DISTANCES FOR SIGNS ARE MEASURED TO THE EDGE OF SIGN NEAREST TO THE RUNWAY OR TAXIWAY EDGE.
  6. THE CONTRACTOR WILL BE REQUIRED TO PROMPTLY REPAIR CONSTRUCTION DAMAGED PAVEMENT ADJACENT TO PROJECT LIMITS.
  7. THE CONTRACTOR WILL BE REQUIRED TO MEGGER TEST ALL EXISTING CIRCUITS BEFORE AND AFTER CONSTRUCTION TO ENSURE PROPER INSULATION AFTER INSTALLATION. MEGGER TESTS MUST BE WITNESSED BY ENGINEER, AND WRITTEN TEST RESULTS MUST BE ALSO SUBMITTED TO ENGINEER.
  8. ANY LIGHTING EQUIPMENT, CIRCUITS, COUNTERPOISE, OR AIRPORT FACILITY DAMAGED AS A RESULT OF THIS PROJECT SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE ENGINEER.
  9. ALL ITEMS REMOVED AS PART OF THIS PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND REMOVED OFF OF AIRPORT PROPERTY UNLESS OTHERWISE NOTED.

10. THE EXISTING ELECTRICAL CABLE, DUCTS, SIGNS, LIGHTS, HANDHOLES, AND THE OTHER ELECTRICAL COMPONENTS ARE SHOWN BASED ON AS-BUILT DRAWINGS AND OTHER REFERENCES. THE LOCATIONS ARE ONLY APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE TO LOCATE THE EXACT LOCATIONS PRIOR TO CONSTRUCTION AND AVOID EXISTING CABLES AND UTILITIES DURING CONSTRUCTION. THE CONTRACTOR MUST COORDINATE THEIR ACTIVITIES WITH THE FAA, AIRPORT AND/OR RESPECTIVE UTILITY COMPANY TO ENSURE THAT SERVICE IS NOT INTERRUPTED.
11. CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF THEY BELIEVE THERE IS A CONTRACTIBILITY CONFLICT BETWEEN SIGN AND LIGHT BASES. CONFLICTS SHALL BE RESOLVED ON A CASE-BY-CASE BASIS.
12. COUNTERPOISE TO BE INSTALLED PER DETAIL ON SHEET E-601. GROUND RODS SHOWN FOR REFERENCE AND SHALL BE INSTALLED AT LOCATIONS STATED IN FAA ITEM L-108 SPECIFICATION.



X:\1821200\221217\01\TECH\CAD\E-202 ELECTRICAL LAYOUT PLAN.DWG  
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## ROSEBURG REGIONAL AIRPORT TAXIWAY A EXTENSION 3896 NW STEWART PARKWAY ROSEBURG, OR 97470

ISSUED FOR BID

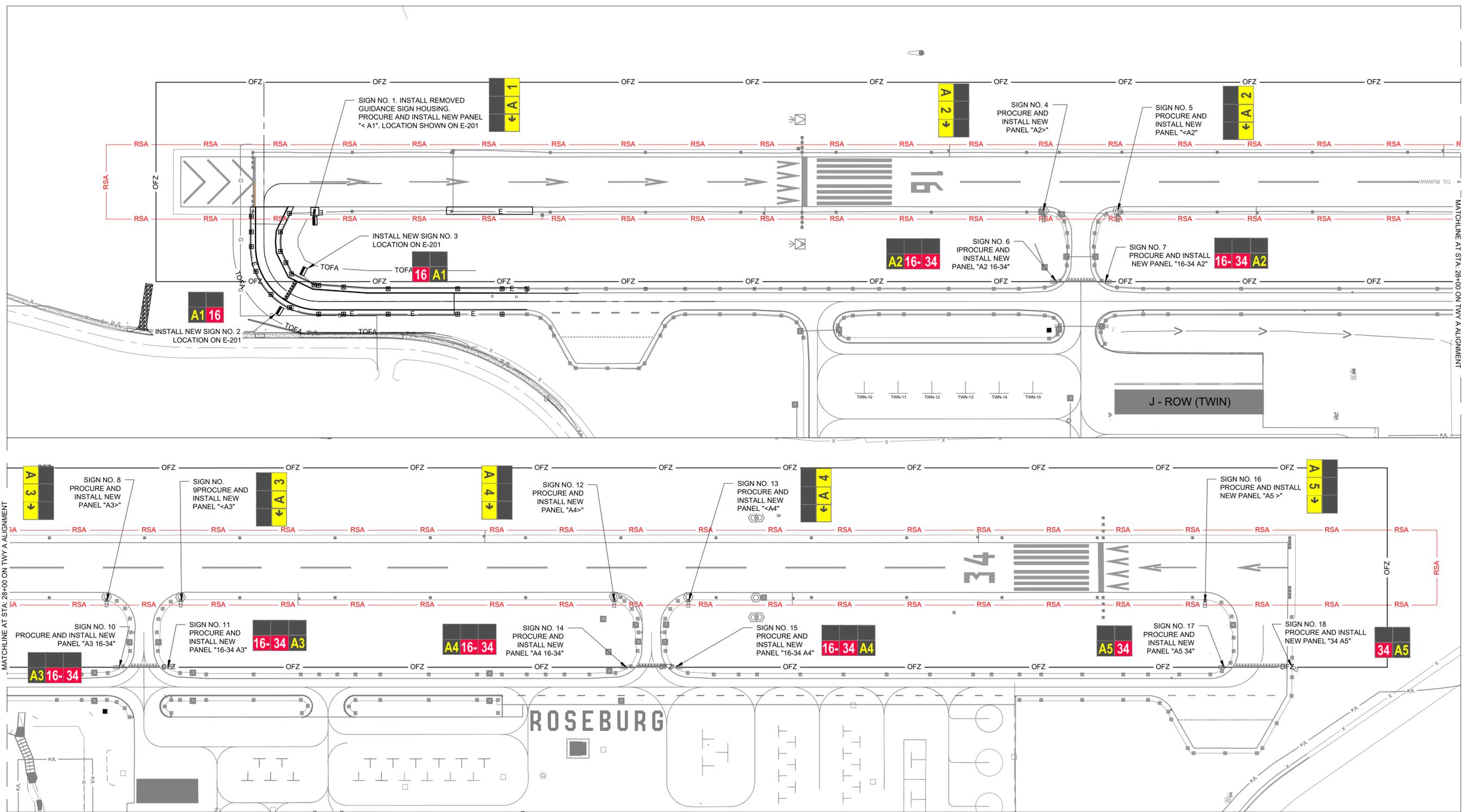


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SHEET CONTENTS  
ELECTRICAL LAYOUT  
PLAN

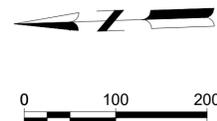
SHEET NO. 33 of 40

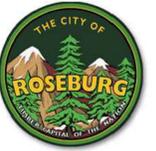
# E-202



### NOTES:

1. THE STATION / OFFSETS IDENTIFIED ON THIS SHEET ARE REFERENCED OFF THE TAXIWAY A ALIGNMENT.
2. SEE SIGN TABLE ON SHEET E-321 FOR MORE DETAILS.





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**ROSEBURG REGIONAL AIRPORT**  
**TAXIWAY A EXTENSION**  
 3896 NW STEWART PARKWAY  
 ROSEBURG, OR 97470

ISSUED FOR BID



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SHEET CONTENTS  
**ELECTRICAL DETAILS**

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## E-321

### GUIDANCE SIGN SCHEDULE

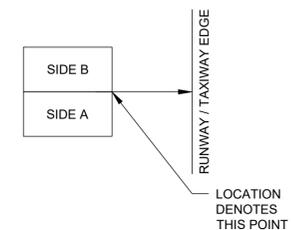
SIGN NUMBER	LOCATION	SIDE A (PRIMARY SIDE)								SIDE B								SPECIFICATIONS				REMARKS
		LEGEND 1	COLOR	LEGEND 2	COLOR	LEGEND 3	COLOR	LEGEND 4	COLOR	LEGEND 1	COLOR	LEGEND 2	COLOR	LEGEND 3	COLOR	LEGEND 4	COLOR	FAA TYPE	SIZE	STYLE	CLASS	
1	RUNWAY 16-34	←	B / Y	A1	B / Y					BLANK	B	BLANK	B					L-858	1	2	2	INSTALL SALVAGED SIGN HOUSING ON NEW BASE. NEW PANEL SIDE A
2	TAXIWAY A1	A1	Y / B	16	W / R					BLANK	B	BLANK	B					L-858	1	2	2	NEW SIGN ON NEW BASE
3	TAXIWAY A	BLANK	B	BLANK	B					16	W / R	A1	Y / B					L-858	1	2	2	NEW SIGN ON NEW BASE
4	RUNWAY 16-34	BLANK	B	BLANK	B					A2	B / Y	→	B / Y					L-858	1	2	2	NEW PANEL SIDE B
5	RUNWAY 16-34	←	B / Y	A2	B / Y					BLANK	B	BLANK	B					L-858	1	2	2	NEW PANEL SIDE A
6	TAXIWAY A2	A2	Y / B	16-	W / R	34	W / R			BLANK	B	BLANK	B	BLANK	B			L-858	1	2	2	NEW PANEL SIDE A
7	TAXIWAY A2	BLANK	B	BLANK	B	BLANK	B			16-	W / R	34	W / R	A2	Y / B			L-858	1	2	2	NEW PANEL SIDE B
8	RUNWAY 16-34	BLANK	B	BLANK	B					A3	B / Y	→	B / Y					L-858	1	2	2	NEW PANEL SIDE B
9	RUNWAY 16-34	←	B / Y	A3	B / Y					BLANK	B	BLANK	B					L-858	1	2	2	NEW PANEL SIDE A
10	TAXIWAY A3	A3	Y / B	16-	W / R	34	W / R			BLANK	B	BLANK	B	BLANK	B			L-858	1	2	2	NEW PANEL SIDE A
11	TAXIWAY A3	BLANK	B	BLANK	B	BLANK	B			16-	W / R	34	W / R	A3	Y / B			L-858	1	2	2	NEW PANEL SIDE B
12	RUNWAY 16-34	BLANK	B	BLANK	B					A4	B / Y	→	B / Y					L-858	1	2	2	NEW PANEL SIDE B
13	RUNWAY 16-34	←	B / Y	A4	B / Y					BLANK	B	BLANK	B					L-858	1	2	2	NEW PANEL SIDE A
14	TAXIWAY A4	A4	Y / B	16-	W / R	34	W / R			BLANK	B	BLANK	B	BLANK	B			L-858	1	2	2	NEW PANEL SIDE A
15	TAXIWAY A4	BLANK	B	BLANK	B	BLANK	B			16-	W / R	34	W / R	A4	Y / B			L-858	1	2	2	NEW PANEL SIDE B
16	RUNWAY 16-34	BLANK	B	BLANK	B					A5	B / Y	→	B / Y					L-858	1	2	2	NEW PANEL SIDE B
17	TAXIWAY A5	A5	Y / B	34	W / R					BLANK	B	BLANK	B					L-858	1	2	2	NEW PANEL SIDE A
18	TAXIWAY A5	BLANK	B	BLANK	B					34	W / R	A5	Y / B					L-858	1	2	2	NEW PANEL SIDE B

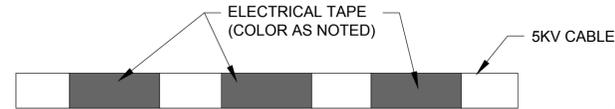
#### KEY TO SIGN SCHEDULE

- $\frac{B}{Y}$  =BLACK LEGEND ON YELLOW BACKGROUND
- $\frac{W}{B}$  =WHITE LEGEND ON BLACK BACKGROUND
- $\frac{Y}{B}$  =YELLOW LEGEND ON BLACK BACKGROUND
- $\frac{B}{B}$  =BLANK BLACK BACKGROUND, NO LETTERING
- $\frac{W}{R}$  =WHITE LEGEND ON RED BACKGROUND

#### SIGN LEGEND NOTES:

- THE SIGN SCHEDULE PROVIDES THE DESCRIPTIONS AND LEGENDS FOR THE ENTIRE SIGN REGARDLESS IF A SIGN IS EXISTING AND TO BE MODIFIED, RELOCATED, OR ALL NEW. REFER TO THE PLAN DRAWINGS AND THIS TABLE TO DETERMINE THE DISPOSITION FOR EACH SIGN.
- "PRIMARY SIDE" (SIDE A) MEANS THE POWER LEG SIDE OF THE SIGN IS ON THE RIGHT AS YOU APPROACH THE SIGN ALONG A RUNWAY OR TAXIWAY CENTERLINE, AND THE SIGN IS ON THE LEFT. "SECONDARY SIDE" (SIDE B) IS THE OPPOSITE SIDE.
- STATION AND OFFSETS LISTED IN THE SIGN LEGEND REFERENCE THE TAXIWAY A ALIGNMENT.

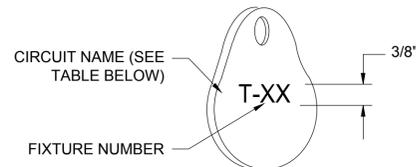




NOTE: APPLY ELECTRICAL TAPE MARKINGS ON EITHER SIDE OF A SPLICE WITHIN A JUNCTION STRUCTURE AND AT A MINIMUM OF 2 LOCATIONS WITHIN A JUNCTION STRUCTURE (JUNCTION CAN AND PULLBOXES).

CIRCUIT NAME	COLOR
RWY	WHITE
TWY	BLUE

**2 CABLE CIRCUIT IDENTIFICATION MARKINGS**  
NO SCALE

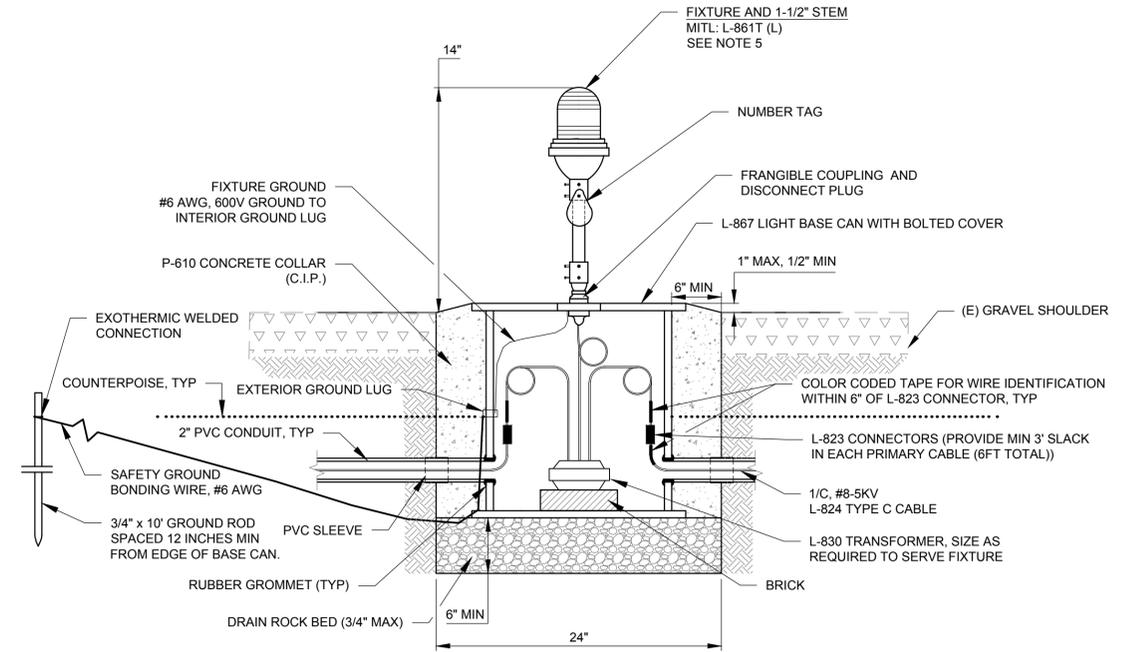


- NOTES:
1. SECURELY AFFIX NON-CORROSIVE NUMBERING TAG TO FIXTURE FACING RUNWAY/TAXIWAY WITH SET SCREW, WIRE TIE, OR METAL BAND. NUMBERS SHALL BE ENGRAVED FOR PERMANENT READABILITY. "T-" FOR TAXIWAY. "XX" REFERS TO LIGHT NUMBER. SEE TAXIWAY EDGE LIGHT LOCATION TABLE.
  2. CONTRACTOR SHALL VERIFY TAGS DO NOT MOVE EITHER TO BE UNREADABLE OR TO BE FATIGUED BY MOVING IN THE WIND.

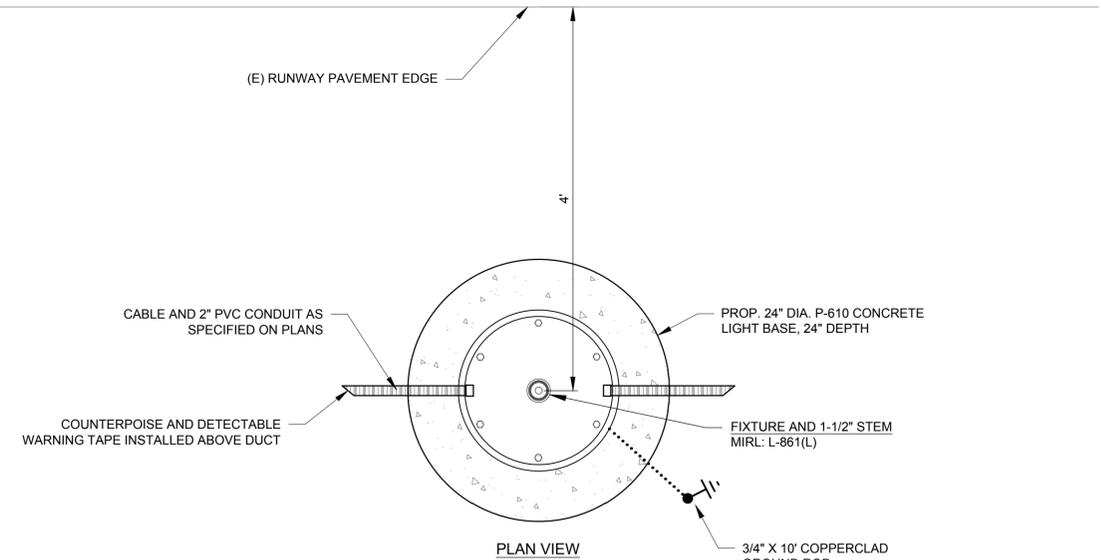
**3 LIGHT FIXTURE IDENTIFICATION TAG**  
NO SCALE

**GENERAL NOTES**

1. SEE PLAN DRAWINGS FOR EDGE LIGHT AND NUMBERING TAG LOCATIONS. CONTRACTOR TO VERIFY FIXTURE TAG NUMBERS WITH THE AIRPORT.
2. NON-METALLIC COMPONENTS IN THE LIGHT FIXTURE STEMS AND BREAKABLE COUPLINGS ARE NOT PERMITTED. L-867 PLASTIC CANS ARE ACCEPTABLE, IF THE METAL THREADED FITTINGS ARE SET IN THE FLANGE DURING THE CASTING PROCESS AND THE CONSTANT CURRENT CIRCUIT IS NOT GROUNDED.
3. ALL HARDWARE SHALL BE STAINLESS STEEL. ALL MACHINE THREADED CONNECTIONS SHALL HAVE ANTI-SEIZING COMPOUND.



- NOTES:
1. LIGHTS AND BASES SHALL BE SET PLUMB AND HORIZONTALLY ALIGNED WITHIN  $\pm 1"$  OF TAXIWAY CENTERLINE.
  2. PROVIDE 3 FT OF CABLE SLACK IN BASE CAN FOR EACH CABLE (6FT TOTAL).
  3. SEE PLAN SET FOR ACTUAL NUMBER OF CABLES.
  4. ALL HARDWARE SHALL BE STAINLESS STEEL. ALL MACHINE THREADED CONNECTIONS SHALL HAVE ANTI-SEIZING COMPOUND.
  5. BLUE FOR TAXIWAY EDGE LIGHTS. RED/RED FOR TEMPORARY RELOCATED THRESHOLD LIGHTS.



**1 ELEVATED EDGE LIGHT**  
NOT TO SCALE



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SHEET CONTENTS  
**ELECTRICAL DETAILS**

SHEET NO. 35 of 40



ABBREVIATIONS:

A.B.	ANCHOR BOLT
ABC	AGGREGATE BASE COURSE
A.C.	ASPHALTIC CONCRETE
ACI	AMERICAN CONCRETE INSTITUTE
AISC	AMERICAN INSTITUTE OF STEEL CONSTRUCTION
ALT	ALTERNATE
ARCH	ARCHITECT OR ARCHITECTURAL DRAWINGS
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
BLDG	BUILDING
BLKG	BLOCKING
BM	BEAM
BOT	BOTTOM
BRG	BEARING
CIP	CAST IN PLACE
CJ	CONTROL JOINT
CL	CENTER LINE
CLR	CLEAR
CMU	CONCRETE MASONRY UNIT
COL	COLUMN
CONC	CONCRETE
CONST JT	CONSTRUCTION JOINT
CONT.	CONTINUOUS
COORD.	COORDINATE
CY	CUBIC YARD
d OR db	DIAMETER OF BAR
DET	DETAIL
DIA	DIAMETER
DIAG	DIAGONAL
DIM	DIMENSION
DL	DEAD LOAD
DWG	DRAWING
EA	EACH
ELEC	ELECTRICAL DRAWINGS
ELEV	ELEVATION
ENCL	ENCLOSE (URE)
ENGR	ENGINEER
EQ	EQUAL
E.S.	EACH SIDE
EXP	EXPANSION
EXST OR (E)	EXISTING
EXT	EXTERIOR
F.B.O.	FURNISHED BY OTHERS
FIN	FINISH
FND	FOUNDATION
FOS	FACE OF STRUCTURE
F.P.	FULL PENETRATION
FTG	FOOTING
GA	GAGE (GAUGE)
GALV.	GALVANIZED

IBC	INTERNATIONAL BUILDING CODE
INT	INTERIOR
JST	JOIST
K.O.	KNOCK OUT
L	ANGLE
LB	POUND
LL	LIVE LOAD
LOC	LOCATION
MATL	MATERIAL
MAX	MAXIMUM
MFR	MANUFACTURER
MIN	MINIMUM
MISC	MISCELLANEOUS
MOD	MODULAR
N.I.C.	NOT IN CONTRACT
NOM	NOMINAL
NTS	NOT TO SCALE
O.C.	ON CENTER
OSSC	OREGON STRUCTURAL SPECIALTIES CODE
P	PIPE
PAF	POWDER ACTUATED FASTENER
PL	PLATE
PSF	POUNDS PER SQUARE FT.
P.P.	PARTIAL PENETRATION
REINF	REINFORCING STEEL
R.O.	ROUGH OPENING
SQ.	SQUARE
S.V.	SHEAR WALL
T&B	TOP & BOTTOM
TS	TUBE STEEL
W OR WF	WIDE FLANGE
WT.	WEIGHT
W/	WITH

GENERAL

- THE CONTRACT STRUCTURAL DRAWINGS AND SPECIFICATIONS REPRESENT THE FINISHED STRUCTURE. THEY DO NOT INDICATE THE METHOD OF CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE ALL MEASURES NECESSARY TO PROTECT THE STRUCTURE DURING CONSTRUCTION. SUCH MEASURES SHALL INCLUDE, BUT NOT BE LIMITED TO BRACING, SHORING FOR LOADS DUE TO CONSTRUCTION EQUIPMENT, ETC. OBSERVATION VISITS TO THE SITE BY THE STRUCTURAL ENGINEER SHALL NOT INCLUDE INSPECTION OF THE ABOVE ITEMS.
- THE CONTRACTOR SHALL VERIFY DIMENSIONS AND ALL EXISTING CONDITIONS SHOWN ON THE DRAWINGS IN THE FIELD AND NOTIFY ENGINEER OF ANY DISCREPANCIES FOR CORRECTION OR VERIFICATION PRIOR TO CONSTRUCTION OF THE AFFECTED WORK. THE COST OF ADDITIONAL DESIGN WORK DUE TO ERRORS OR OMISSIONS IN CONSTRUCTION SHALL BE BORNE BY THE CONTRACTOR.
- ESTABLISH AND VERIFY ALL OPENINGS AND INSERTS FOR MECHANICAL, ELECTRICAL AND PLUMBING WITH APPROPRIATE TRADES AND THE DRAWINGS.
- PROVIDE ALL NECESSARY TEMPORARY BRACING, SHORING, GUYING OR OTHER MEANS TO AVOID EXCESSIVE STRESSES AND TO HOLD STRUCTURAL ELEMENTS IN PLACE DURING CONSTRUCTION.
- DETAILS ON THE DRAWINGS ARE TYPICAL. VERIFY ALL DIMENSIONS.
- DIMENSIONS ON THE STRUCTURAL DRAWINGS ARE EXACT WITH THE EXCEPTION OF MASONRY AND SAWN LUMBER DIMENSIONS WHICH ARE NOMINAL.
- NOTES AND DETAILS ON DRAWINGS SHALL TAKE PRECEDENCE OVER GENERAL NOTES AND TYPICAL DETAILS. WHERE NO DETAILS ARE SHOWN, CONSTRUCTION SHALL CONFORM TO SIMILAR WORK ON THE PROJECT.
- WHERE REFERENCE IS MADE TO VARIOUS TEST STANDARDS FOR MATERIALS, SUCH STANDARDS SHALL BE THE LATEST EDITION AND/OR ADDENDUM.
- DRAWINGS AND SPECIFICATIONS ARE INSTRUMENTS OF SERVICE IN RESPECT TO THIS SPECIFIC PROJECT AND ARE NOT INTENDED OR REPRESENTED TO BE SUITABLE FOR REUSE ON EXTENSIONS OF THIS PROJECT OR ON ANY OTHER PROJECT ANY REUSE WITHOUT WRITTEN VERIFICATION OR ADAPTATION BY ENGINEER WILL BE AT OWNER'S SOLE RISK AND WITHOUT LIABILITY OR LEGAL EXPOSURE TO ENGINEER. OWNER SHALL INDEMNIFY AND HOLD HARMLESS ENGINEER FROM ANY AND ALL CLAIMS, DAMAGES, LOSSES AND EXPENSES INCLUDING ATTORNEY'S FEES ARISING OUT OF OR RESULTING FROM UNAUTHORIZED REUSE.
- NO CHANGES FROM THE APPROVED STRUCTURAL PLANS SHALL BE MADE IN THE FIELD UNLESS, PRIOR TO MAKING CHANGES, WRITTEN APPROVAL IS OBTAINED FROM THE ENGINEER. IF CHANGES ARE MADE WITHOUT WRITTEN APPROVAL SUCH CHANGES SHALL BE THE LEGAL AND FINANCIAL RESPONSIBILITY OF THE CONTRACTOR OR SUB-CONTRACTORS INVOLVED AND IT SHALL BE THEIR RESPONSIBILITY TO REPLACE OR REPAIR THE CONDITION AS DIRECTED BY THE ENGINEER.
- ENGINEERING DESIGN PROVIDED BY OTHERS AND SUBMITTED FOR REVIEW SHALL BEAR THE SEAL AND SIGNATURE OF A PROFESSIONAL ENGINEER REGISTERED IN OREGON.
- USE OF THESE PLANS BY THE CONTRACTOR CONSTITUTES ACCEPTANCE OF THESE NOTES AND CONDITIONS.
- THE RETAINING WALL DESIGN IS BASED ON JET BLAST LOADS OF 50PSF. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF ANY REVISIONS REQUIRED.
- THE SIZE, NUMBER, AND PLACEMENT OF ANCHOR BOLTS FOR THE BLAST FENCE IS TO BE DETERMINED BY THE BLAST FENCE MANUFACTURER. CONTRACTOR IS REQUIRED TO COORDINATE ANCHORAGE LOCATIONS WITH SUPPLEMENTAL WALL REINFORCEMENT REQUIRED.
- CONTRACTOR IS TO PROVIDE FINAL BLAST FENCE DESIGN LOADS TO THE ENGINEER FOR REVIEW PRIOR TO WORK.

STRUCTURAL NOTES  
CODES

- 2022 OREGON STRUCTURAL SPECIALTIES CODE
- ACI 318-19

DESIGN LOADS

- IMPACT LOAD TO BLAST DEFLECTOR PROVIDED BY BLAST DEFLECTORS, INC. (BDI)

CONCRETE NOTES AND REINFORCING STEEL NOTES

MATERIAL PROPERTIES:  
COMPRESSIVE STRENGTH, F'c = 4.5 KSI  
CONCRETE REINFORCEMENT, Fy = 60 KSI (A615 GR 60)

- FOOTING SUBGRADES SHALL BE CLEAN AND FREE OF DEBRIS, STANDING WATER, AND LOOSE SOIL.
- ALL BAR LAPS SHALL COMPLY WITH ACI 318, CLASS "B" SPLICE CRITERIA.
- LAP LENGTH SHALL BE SPECIFICALLY NOTED ON SHOP DRAWINGS WHERE MORE THAN ONE BAR MAKES UP A CONTINUOUS STRING.

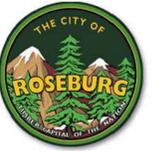
SCOPE OF SPECIAL INSPECTIONS

THE FOLLOWING REQUIRES SPECIAL INSPECTION:

ITEM	FREQUENCY
EXCAVATED SOIL	AFTER EXCAVATION
CAST IN PLACE CONCRETE	CONTINUOUS DURING PLACEMENT, SAMPLES SHALL BE TESTED FOR SLUMP, AIR CONTENT, TEMPERATURE AND COMPRESSIVE STRENGTH PER CURRENT ASTM STANDARDS.
REINFORCING STEEL	AFTER PLACEMENT



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Portland, OR 97220  
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ROSEBURG REGIONAL AIRPORT  
TAXIWAY A EXTENSION  
3896 NW STEWART PARKWAY  
ROSEBURG, OR 97470

ISSUED FOR BID



AIP NO: 3-41-0054-030-2024  
MSH NO: 1821200-221217.01  
DATE: APRIL 2024  
DESIGNED BY: MLF  
DRAWN BY: MJK  
CHECKED BY: RJB  
DO NOT SCALE DRAWINGS

SHEET CONTENTS  
GENERAL RETAINING  
WALL STRUCTURAL  
NOTES

SHEET NO. 37 of 40

S-001



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**ROSEBURG REGIONAL AIRPORT**  
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 3896 NW STEWART PARKWAY  
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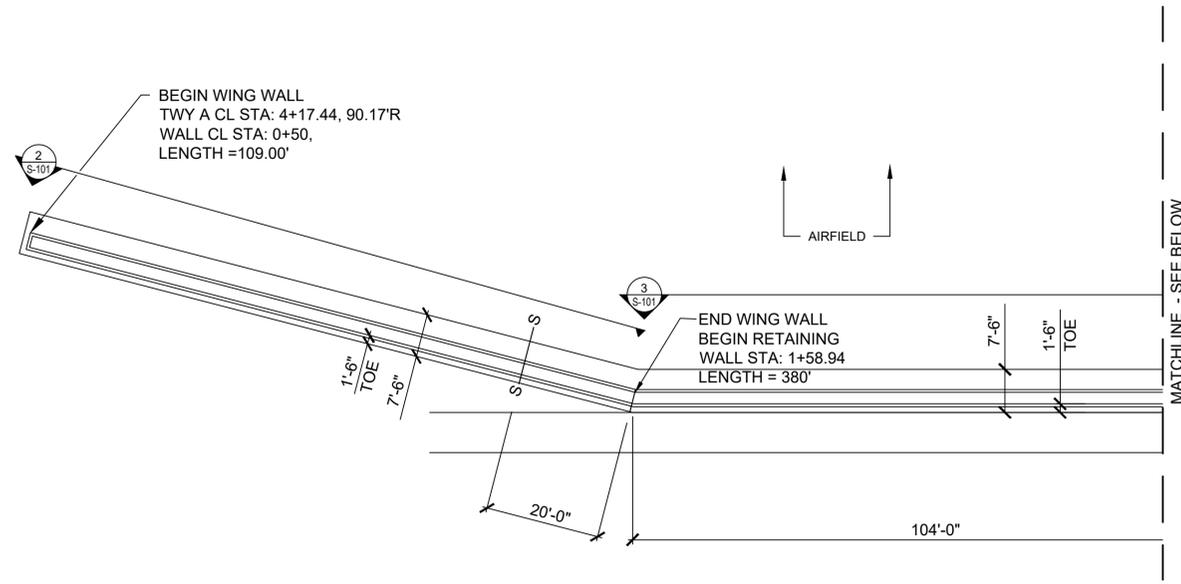


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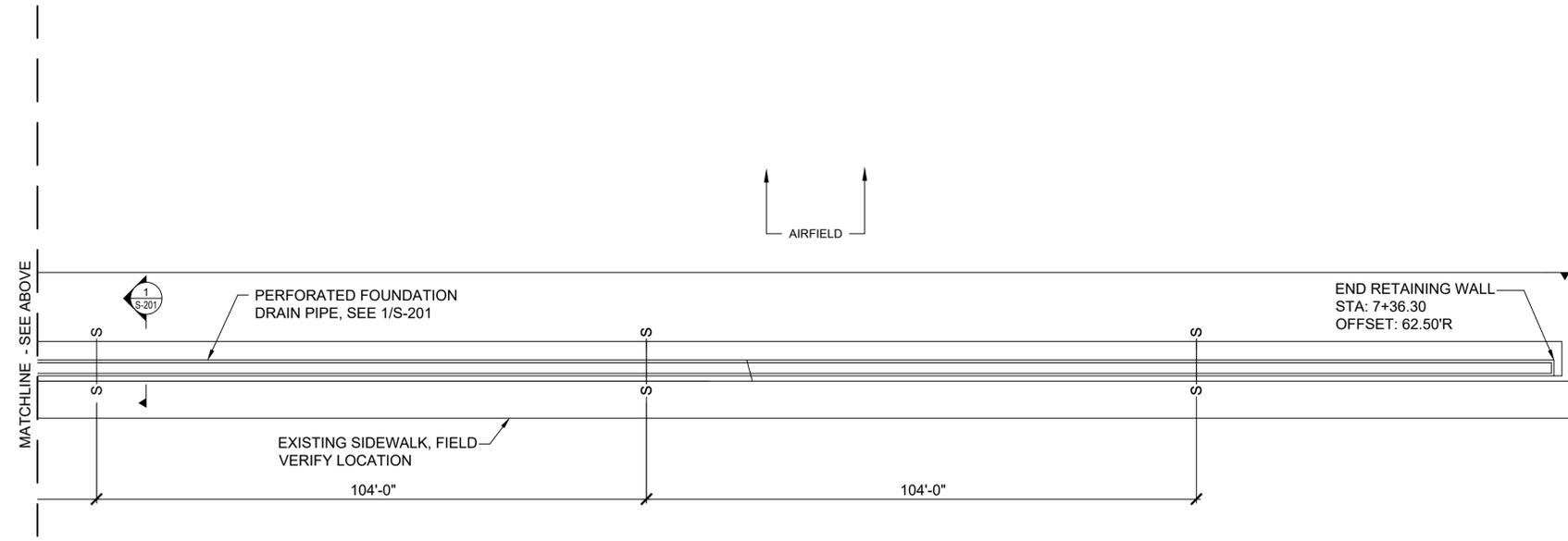
SHEET CONTENTS  
 RETAINING WALL  
 PLAN & ELEVATIONS

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## S-101



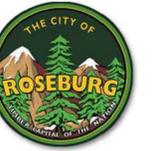
**1 WING WALL AND RETAINING WALL PLAN**  
 S-101 SCALE: 1/16" = 1'-0"



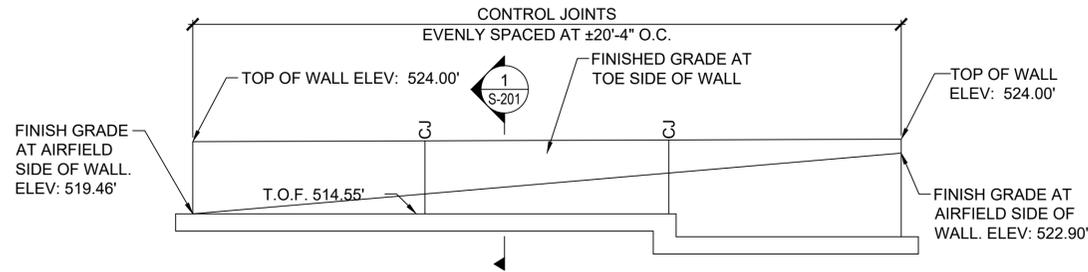
**1 WING WALL AND RETAINING WALL PLAN**  
 S-101 SCALE: 1/16" = 1'-0"

- LEGEND**
- S—S STEPPED FOOTING LOCATION, SEE 5/S-201
  - EJ EXPANSION JOINT, SEE 7/S-201
  - CJ CONTROL JOINT, SEE 8/S-201
- NOTES:**
1. SEE S-201 FOR TYPICAL CONCRETE DETAILS.
  2. WHERE FIBER OPTICS LINE IS ENCOUNTERED PROVIDE SPLIT STEEL PIPE ENCASEMENT PER 6/S-201
  3. STATIONING AND OFFSET ARE AT FRONT OF WALL. COORDINATE DIMENSIONS WITH C-101

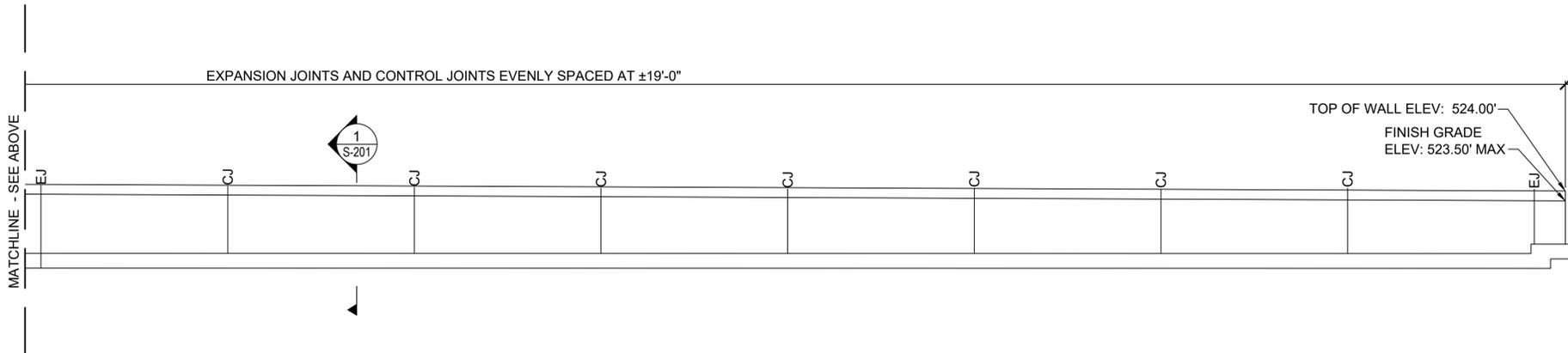
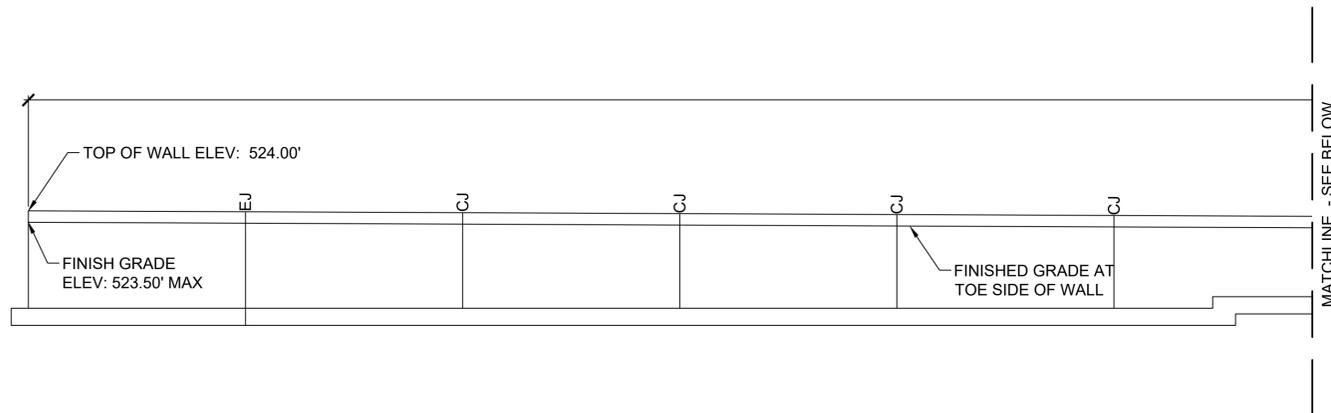
X:\1821200\221217.01\TECH\CAD\S-102 RETAINING WALL PLAN & ELEVATIONS.DWG  
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**2 NORTH WING WALL - AIRFIELD SIDE**  
 SCALE: 1/8" = 1'-0"



**3 RETAINING WALL - AIRFIELD SIDE**  
 SCALE: 1/8" = 1'-0"

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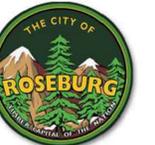


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SHEET CONTENTS  
 RETAINING WALL  
 PLAN & ELEVATIONS

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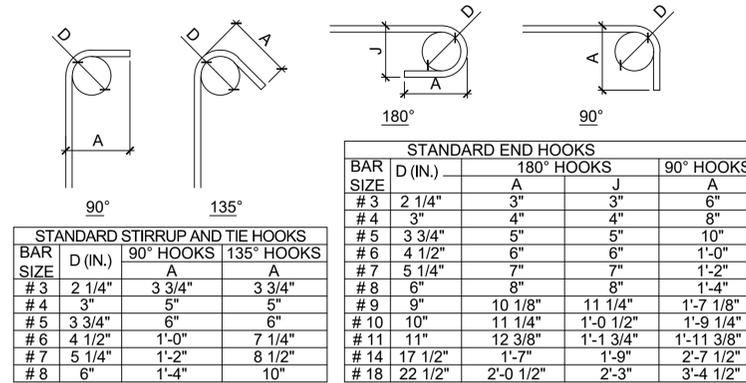
**S-102**



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MINIMUM REINFORCING BAR LAP SPLICE LENGTH	
	LENGTH
#3	23"
#4	31"
#5	38"
#6	46"
#7	67"
#8	76"
#9	86"
#10	95"
#11	104"
MINIMUM REINFORCING BAR LAP SPLICE LENGTH	
CONDITION	COVER
CONCRETE CAST ON EARTH	3"
CONC. EXPOSED TO EARTH OR WEATHER	
#6 OR LARGER	2"
#5 OR SMALLER	1 1/2"
CONC. NOT EXPOSED TO EARTH OR WEATHER, SLABS, WALLS, OR JOISTS	
#14 OR #18	1 1/2"
#11 OR SMALLER	3/4"
BEAMS OR COLUMNS	1 1/2"

NOTE: SEE DRAWINGS FOR OTHER SPECIFIC COVER REQUIREMENTS

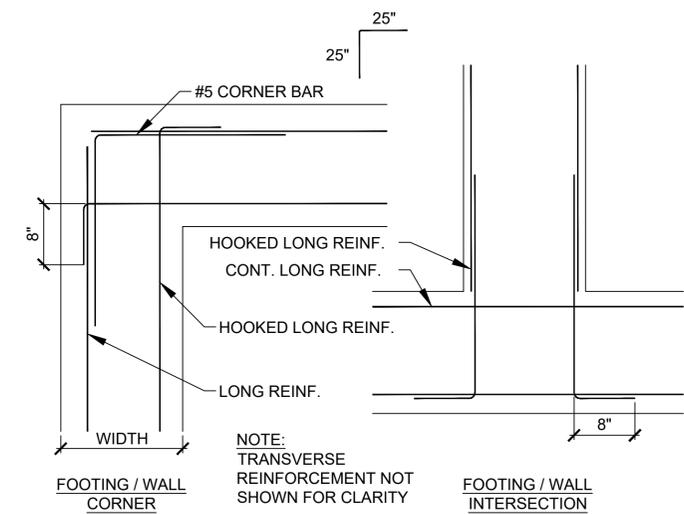


STANDARD END HOOKS				
BAR SIZE	D (IN.)	180° HOOKS		90° HOOKS
		A	J	A
#3	2 1/4"	3"	3"	6"
#4	3"	4"	4"	8"
#5	3 3/4"	5"	5"	10"
#6	4 1/2"	6"	6"	1'-0"
#7	5 1/4"	7"	7"	1'-2"
#8	6"	8"	8"	1'-4"
#9	9"	10 1/8"	11 1/4"	1'-7 1/8"
#10	10"	11 1/4"	1'-0 1/2"	1'-9 1/4"
#11	11"	12 3/8"	1'-1 3/4"	1'-11 3/8"
#14	17 1/2"	1'-7"	1'-9"	2'-7 1/2"
#18	22 1/2"	2'-0 1/2"	2'-3"	3'-4 1/2"

STANDARD STIRRUP AND TIE HOOKS				
BAR SIZE	D (IN.)	90° HOOKS		135° HOOKS
		A	A	A
#3	2 1/4"	3 3/4"	3 3/4"	3 3/4"
#4	3"	5"	5"	5"
#5	3 3/4"	6"	6"	6"
#6	4 1/2"	1'-0"	7 1/4"	7 1/4"
#7	5 1/4"	1'-2"	8 1/2"	8 1/2"
#8	6"	1'-4"	10"	10"

2 REINFORCING BAR BENDS

S-201 SCALE: 1 1/2" = 1'-0"

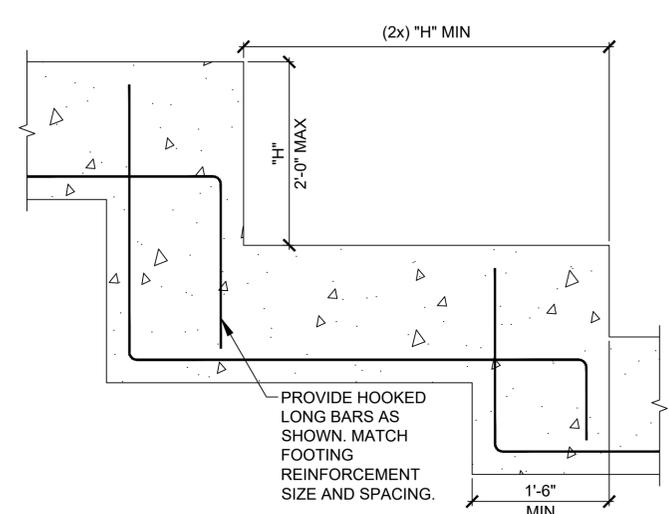


4 TYPICAL FOOTING AND WALL REINF.

S-201 SCALE: 1" = 1'-0"

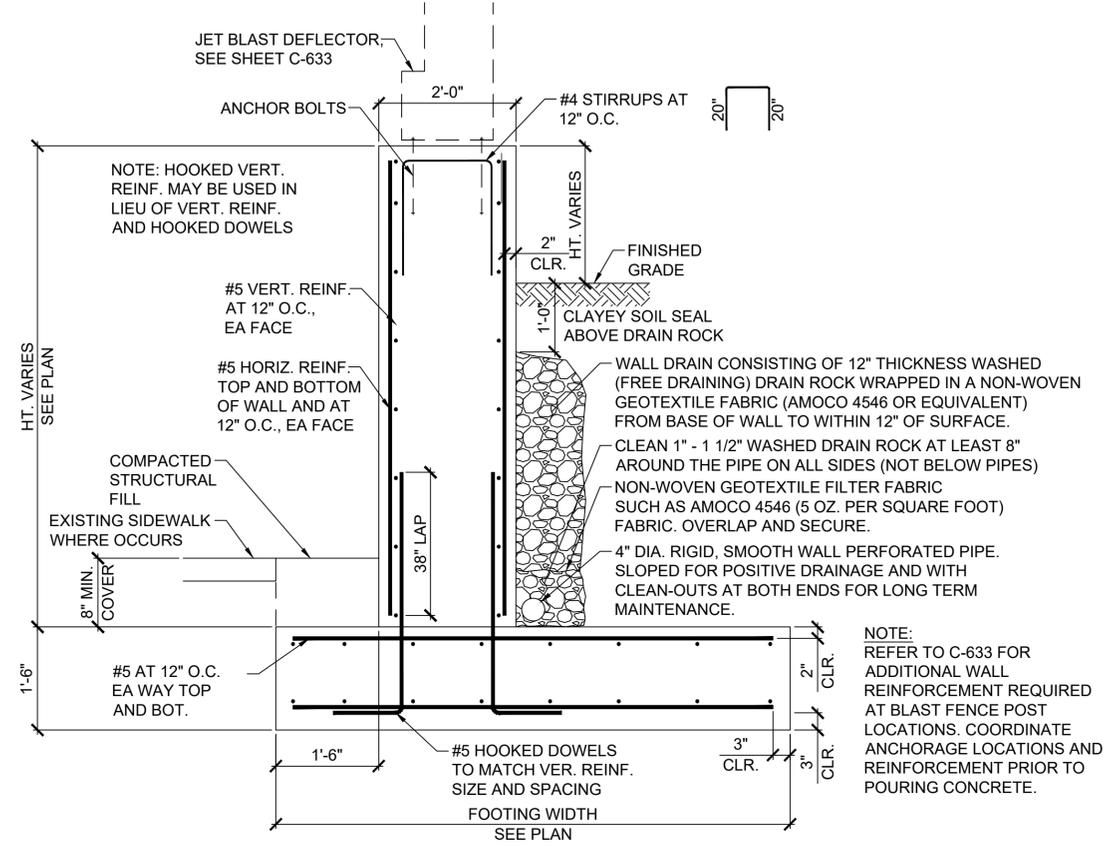
3 TYPICAL LAP SPLICE AND COVER

S-201 SCALE: 1" = 1'-0"



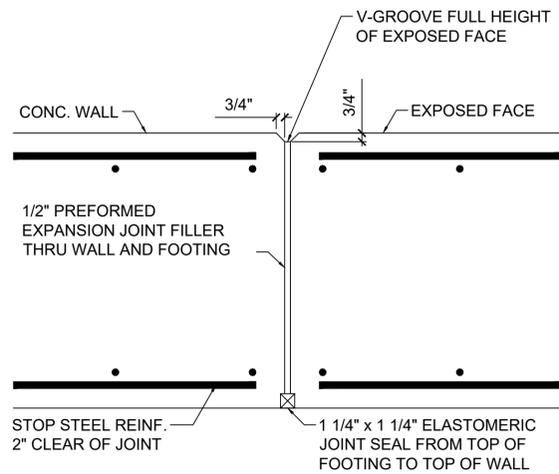
5 TYPICAL STEPPED FOOTING REINF.

S-201 SCALE: 1" = 1'-0"



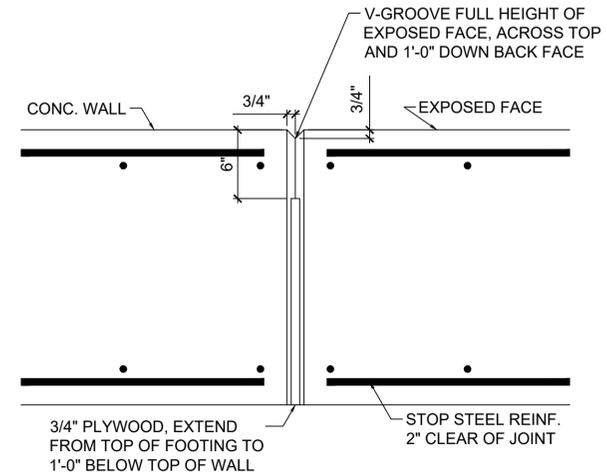
1 TYPICAL FOOTING

S-201 SCALE: 3/4" = 1'-0"



7 EXPANSION JOINT

S-201 SCALE: 1 1/2" = 1'-0"



8 CONTROL JOINT - CJ

S-201 SCALE: 1 1/2" = 1'-0"

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SHEET CONTENTS  
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X:\1821200\221217\_01\TECH\CAD\S-201 RETAINING WALL STRUCTURAL DETAILS.DWG 4/11/2024 12:25:09 PM